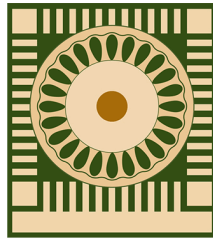


CITY OF HAYWARD

Hayward City Hall
777 B Street
Hayward, CA 94541
www.Hayward-CA.gov



CITY OF
HAYWARD
HEART OF THE BAY

Agenda

Thursday, July 12, 2018

7:00 PM

Council Chambers

Planning Commission

MEMBERS OF THE AUDIENCE WISHING TO ADDRESS THE PLANNING COMMISSION

Obtain a speaker’s identification card, fill in the requested information, and give the card to the Commission Secretary. The Secretary will give the card to the Commission Chair who will call on you when the item in which you are interested is being considered. When your name is called, walk to the rostrum, state your name and address for the record and proceed with your comments. The Chair may, at the beginning of the hearing, limit testimony to three (3) minutes per individual and five (5) minutes per an individual representing a group of citizens for organization. Speakers are expected to honor the allotted time.

CALL TO ORDER Pledge of Allegiance

ROLL CALL

PUBLIC COMMENTS

The PUBLIC COMMENTS section provides an opportunity to address the Planning Commission on items not listed on the agenda. The Commission welcomes your comments and requests that speakers present their remarks in a respectful manner, within established time limits and focus on issues which directly affect the City or are within the jurisdiction of the City. As the Commission is prohibited by State law from discussing items not listed on the agenda, your item will be taken under consideration and may be referred to staff for further action.

ACTION ITEMS

The Commission will permit comment as each item is called for Public Hearing. Please submit a speaker card to the Secretary if you wish to speak on a public hearing item.

PUBLIC HEARING

For agenda item No. 1, the decision of the Planning Commission is final unless appealed. The appeal period is 10 days from the date of the decision. If appealed, a public hearing will be scheduled before the City Council for final decision.

For agenda item No. 2 the Planning Commission may make a recommendation to the City Council.

1. [PH 18-052](#) Proposed Establishment of a Car Wash and Expanded Convenience Store at an Existing Gas Station at 438 West Tennyson Road (Assessor Parcel No. 465-0001-002-00) from Kevin Chiang, KC Associates/Xu Ying and Fan Li (Applicant/Owners) Requiring Approval of Conditional Use Permit Application No. 201601868.

- Attachments:** [Attachment I Staff Report](#)
 [Attachment II Findings](#)
 [Attachment III Conditions of Approval](#)
 [Attachment IV Project Plans](#)

2. [PH 18-059](#) Application to Amend Chapter 10, Article 1(Zoning Ordinance), Sections 10-1.845.j (5) and (6); and 10-2.1045.j(5) and (6) (Minimum Design and Performance Standards) of the Hayward Municipal Code Related to Drive-Through Restaurants and Drive-Through Coffee/Esspresso Shops in the City of Hayward by United Growth Capital Management, LLC. (Applicant), Requiring Approval of a Zoning Text Amendment. Application No. 201802227.

Attachments: [Attachment I Staff Report](#)
[Attachment II Findings for Approval](#)
[Attachment III Proposed Text Amendments](#)
[Attachment IV April 2, 2018 CEDC Meeting Minutes](#)
[Attachment V Map of Drive-Through Restaurants](#)
[Attachment VI Map of Half-Mile Buffer from Freeways](#)

COMMISSION REPORTS

Oral Report on Planning and Zoning Matters

Commissioners' Announcements, Referrals

ADJOURNMENT

NEXT MEETING, JULY 26, 2018, 7:00PM

PLEASE TAKE NOTICE

That if you file a lawsuit challenging any final decision on any public hearing item listed in this agenda, the issues in the lawsuit may be limited to the issues which were raised at the City's public hearing or presented in writing to the City Clerk at or before the public hearing.

PLEASE TAKE FURTHER NOTICE

That the City Council has adopted Resolution No. 87-181 C.S., which imposes the 90 day deadline set forth in Code of Civil Procedure section 1094.6 for filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure section 1094.5.

****Materials related to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the Permit Center, first floor at the above address. Copies of staff reports for agenda items are available from the Commission Secretary and on the City's website the Friday before the meeting.****

Assistance will be provided to those requiring accommodations for disabilities in compliance with the Americans with Disabilities Act of 1990. Interested persons must request the accommodation at least 48 hours in advance of the meeting by contacting the City Clerk at (510) 583-4400 or TDD (510) 247-3340.



CITY OF HAYWARD

Hayward City Hall
777 B Street
Hayward, CA 94541
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File #: PH 18-052

DATE: July 12, 2018

TO: Planning Commission

FROM: Planning Manager

SUBJECT

Proposed Establishment of a Car Wash and Expanded Convenience Store at an Existing Gas Station at 438 West Tennyson Road (Assessor Parcel No. 465-0001-002-00) from Kevin Chiang, KC Associates/Xu Ying and Fan Li (Applicant/Owners) Requiring Approval of Conditional Use Permit Application No. 201601868.

RECOMMENDATION

That the Planning Commission approve the Conditional Use Permit based on the analysis set forth in this report and the attached Findings (Attachment II), and subject to the Conditions of Approval (Attachment III).

SUMMARY

The proposed project would result in renovation of the Tennyson Valero service station to replace an existing small convenience store with a new, approximately 3,000 square foot convenience store with offices above, and to install a new self-service car wash within a concrete shell structure. The existing gas station canopy, fuel stations and related underground equipment would remain in place and the service station would continuously operate during construction.

ATTACHMENTS

Attachment I	Staff Report
Attachment II	Findings
Attachment III	Conditions of Approval
Attachment IV	Project Plans



SUBJECT

Proposed Establishment of a Car Wash and Expanded Convenience Store at an Existing Gas Station at 438 West Tennyson Road (Assessor Parcel No. 465-0001-002-00) from Kevin Chiang, KC Associates/Xu Ying and Fan Li (Applicant/Owners) Requiring Approval of Conditional Use Permit Application No. 201601868

RECOMMENDATION

That the Planning Commission approve the Conditional Use Permit based on the analysis set forth in this report and the attached Findings (Attachment II), and subject to the Conditions of Approval (Attachment III).

SUMMARY

The proposed project would result in renovation of the Tennyson Valero service station to replace an existing small convenience store with a new, two-story commercial structure containing an approximately 3,000 square foot convenience store with offices above, and the installation of a new self-service car wash within a concrete shell structure. The existing gas station canopy, fuel stations and related underground equipment would remain in place and the gas station would continuously operate during construction.

BACKGROUND

The service station was built in 1960. In 1973, the Zoning Board of Adjustments approved a Use Permit (Use Permit 73-146) to remodel the station to become a self-service facility. The service station has operated continuously since that time with minor upgrades and modifications to the building, fuel stations and service station canopy.

Public Outreach: An initial Notice of Receipt of the project application was sent to 146 property owners and residents within a 300-foot radius of the site on April 18, 2016. In addition, the application was routed to the Eastwood Homeowners Association and the South Hayward Neighborhood Group.

On June 15, 2018, public hearing notices related to the Conditional Use Permit was published in The Daily Review and sent to all property owners and residents within a 300-foot radius of the project site. To date, no comments have been received on the project.

PROJECT DESCRIPTION

Existing Conditions: The approximately half-acre site is located at 438 West Tennyson Road (Assessor's Parcel Number 465-0001-002-00). The site is flat and currently developed with three fuel pumps covered by an approximately 2,000 square foot canopy, a 980-square foot convenience store, paved drive aisles and haphazard, unmarked parking areas. The convenience store is operated by one employee and sells miscellaneous snacks, drinks, automotive products and tobacco products within a 24-cubic foot area that is accessible to employees only. There are two, two-way driveways (measuring between 40 and 45 feet in width), providing access to the site from West Tennyson Road and small pockets of landscaping that frame the driveways. Approximately 9,000 square feet of the lot along the southern property (adjacent to Tennyson Park) is vacant and undeveloped.

Surrounding uses include commercial uses along north, east and west along Tennyson Road and Tennyson Park to the south of the project site. The Hayward Police South District Office, the Eden Youth and Family Center and the Matt Jimenez Community Center are located approximately five hundred feet west of the project site, and Hayward Fire Station No. 7 is located approximately 500 feet east of the project site.

Proposed Project: The proposed project includes construction of a new two-story, 3,000 square foot commercial structure that would contain a convenience store and offices; construction of a new 1,205 square foot self-service car wash; trash enclosure; and installation of related site improvements. The existing fuel stations and canopy would remain in place and the business would maintain continuous operation during construction.

As proposed, the existing convenience store would be demolished and relocated from the center of the site to the eastern part of the site. The proposed use would be expanded from a single room to a larger, two story structure that would contain a mini-market convenience store with restrooms on the ground floor and two ancillary offices with restrooms above. The entrance and glazing for the convenience store would be located on the western elevation to provide a visual connection to the fuel and car wash areas. The entrance to the store would be framed by a wooden trellis with climbing ivy. The northern and southern elevations would be decorated with a curved standing seam metal roof, color trim, bracing elements and an arched breezeway to create plane breaks in the building. The eastern elevation which would be sited right at the property line would be a one-hour rated concrete masonry wall.

The convenience store would continue to operate 24 hours a day with two in-store employees covering each shift. The mini-market would sell fuel, food and drinks and would continue selling tobacco products as a legal, nonconforming use (see additional analysis under Policy Context and Code Compliance below). The self-serve car wash unit would be located at the rear of the site within a concrete masonry structure that would be painted to match the convenience store. An approximately 100-foot long drive aisle along the western property line would provide ample queuing area for cars waiting to enter the car wash. The car wash would operate from 6 a.m. to 8 p.m. and would be accessed using a self-serve console along the queuing aisle.

Site access would continue to be from two driveways along West Tennyson however the driveways would be reduced in width (from 45 feet to 34 feet in width), and upgraded with driveway aprons that meet City standards. The driveways would be framed with landscaped pockets and the landscaped island between the driveways would be expanded. Overall, approximately 5,553 square feet of the site would be landscaped within front and rear setbacks, pocket landscaped areas at the entrance to the convenience store, and around the parking area/car wash queue aisle. A row of trees and an eight-foot-tall open metal fence are proposed between the project site and Tennyson Park.

Nine 90-degree parking spaces would be located behind the fueling area. The parking spaces would be equipped with stations and covered by a canopy. Two additional parallel parking spaces would be located along the eastern property line in front of the convenience store. Bicycle parking for up to two bicycles will be located at the entrance plaza.

Sustainability Features: The new development will meet the code requirements set forth in the Cal Green Building Code standards in effect when the applicant pulls a building permit.

POLICY CONTEXT AND CODE COMPLIANCE

Hayward 2040 General Plan: The project site has a [Medium Density Residential General Plan](#) land use designation where neighborhood commercial uses, including but not limited to service stations are, considered allowable supporting uses (General Plan 3-14). The proposed expansion of the convenience store and the car wash use is consistent with the policies and guidelines of the *Hayward 2040 General Plan*.

Zoning Ordinance: The project site is located within the [CN \(Neighborhood Commercial\) District](#) where service stations and car washes are subject to Conditional Use Permit approval. Pursuant to [HMC Section 10-1.3205](#), the purpose for requiring conditional use permit approval are to assure certain uses, as specified in the various districts, are permitted where there is a community need, and to assure said uses occur in maximum harmony with the area and in accordance with official City policies. All conditions of approval and all elements of the exhibit approved with the use permit are required to be completed before occupancy or onset of business unless specifically indicated otherwise.

As noted above, the existing service station was approved with Use Permit No. 73-146; therefore, the applicant is seeking the subject Conditional Use Permit to install a car wash at the site and to relocate and expand the convenience store. The proposed convenience store and car wash structures and site improvements are consistent with the required setbacks, lot coverage and height limit for the CN District.

Parking: Pursuant to the [Hayward Municipal Code \(HMC\) Section 10-2.340](#), service stations with convenience stores shall provide one parking space for each employee with the longest shift, one parking space for air/water dispensers, and one parking space for each 400 square feet of gross floor area of the convenience store for a total of eleven parking stalls. In addition, self-service car washes shall provide area for three queuing spaces

(approximately 60 feet in length). The proposed development is consistent with these requirements.

Noise Regulations: Pursuant to [HMC Section 4-1.03.1\(a\)](#), noise generated by a person, device or machine on commercially zoned property shall not exceed 70 dBA at any point outside of the property plane. According to the car wash equipment details, the dryer system component has the highest potential for generating noise that exceeds 70 dBA at the property plane. See further discussion under Staff Analysis below.

Tobacco Retail Sales: Pursuant to [HMC Section 10-1.2780, Tobacco Retail Sales Establishments](#), all tobacco retails shall obtain a Conditional Use Permit to sell tobacco and tobacco related products. Tobacco retailers legally existing prior to the adoption of the regulations may exist without the approval of a Conditional Use Permit but must otherwise comply with all local, state, or federal laws applicable to tobacco products, electronic smoking devices or tobacco paraphernalia, and all requirements and operational standards contained within the City's Ordinance.

According to City records, the subject business was legally established prior to the adoption of the Tobacco Retail Sales Ordinance and has a current Tobacco Retail License. Per [HMC Section 10-1.2915, Nonconforming Uses](#), tobacco retail sales may continue with the proposed redevelopment of the site if the use is not discontinued for a period of six month or longer. If the use is discontinued or if the applicant fails to meet the standards set forth in the Tobacco Retail Sales Ordinance, then the right to engage in tobacco sales may be terminated in accordance with the Ordinance.

Strategic Priority: This agenda item supports the Tennyson Corridor Strategic Initiative. The purpose of the Tennyson Corridor Strategic Initiative is to develop an attractive, cohesive, thriving, Tennyson Corridor through thoughtful engagement of residents, businesses and community partnerships. This item supports Goal 3, Improve Community Appearance and Objectives 3.a to enhance landscaping by expanding the landscaping along the property frontage; 3.b to decrease dumping and littering by utilizing the entire site for the development; and 3.c to decrease blight by redeveloping the site with a new convenience store and car wash structure.

STAFF ANALYSIS

Staff believes that the Commission can make the findings to approve the Conditional Use Permit for the service station remodel and expansion and the installation of a new car wash. Redevelopment of the site will result in construction of a new expanded convenience store, increased site landscaping, a more attractive boundary along Tennyson Park, a more efficient site circulation and parking, and a self-serve car wash that will support public convenience and welfare.

As noted above, the convenience store on the site currently has a Tobacco Retailer's License because tobacco sales were occurring on the site prior to the adoption of the Ordinance. Typically, tobacco sales would not be permitted in the CN District nor would it be permitted

within 500 feet of a public park and community centers; however, as described above, the use is considered a legal non-conforming use if the use is not discontinued for longer than six months; the owner maintains a current Tobacco Retailer License; and, the owner abides by the standards and requirements of the Ordinance (proposed Condition No. 13).

Staff does not believe that the car wash use will be detrimental to public health, safety or general welfare in that the car wash equipment most likely to generate noise impacts that exceed applicable noise standards would be enclosed within a concrete masonry block structure. In addition, the exit of the car wash would be shielded from the property boundaries by the trash enclosure structure and the new convenience store which will significantly reduce noise leakage from the car wash. Staff does recommend that the Commission adopt Condition No. 19 requiring that the applicant install noise suppression equipment or automatic doors at the exit to further buffer any potential noise impacts if noise generated from the car wash exceeds allowable levels.

Staff also recommends design modifications to ensure that the development does not impair the character and integrity of the surrounding area. Specifically, the eastern façade of the convenience store which is visible from the adjacent property, is proposed to be a one-hour rated, plain concrete masonry unit (CMU) wall with little articulation. In addition, the southern elevation of the car wash is slated to be a CMU wall with cement finish despite the fact that it will be visible from Tennyson Park. To ensure that the development is well designed and is well integrated visually from adjacent properties, staff recommends that the Commission adopt Condition Nos. 20 and 21, requiring that the applicant add articulation to the eastern elevation of the convenience store and the southern elevation of the car wash by matching the colors and materials to the other elevations and adding faux windows and trim, a belly band, a mural, mosaic or other artistic elements to the applicable walls.

ENVIRONMENTAL REVIEW

This project is categorically exempt from environmental review in accordance with the California Environmental Quality Act (CEQA) Guidelines, Section 15332, Infill Development Projects, in that the proposed project is consistent with applicable general plan policies and zoning designation and requirements; is located on a half-acre site that is surrounded by development on a site that is current being used as a gas station; is adequately served by utilities and public services; and, has no value as habitat for endangered, rare or threatened species. Redevelopment of the site to expand the convenience store and to add a car wash will not result in any significant effects related to traffic, noise, air quality or water quality.

NEXT STEPS

There will be a 10-day appeal period following the Planning Commission decision on the Conditional Use Permit. If an appeal is not received, the decision will become final.

Prepared by: Leigha Schmidt, Senior Planner

Recommended by: Sara Buizer, Planning Manager

Approved by:



Sara Buizer, AICP, Planning Manager



Stacey Bristow, Interim Development Services Director

**CITY OF HAYWARD
PLANNING COMMISSION
CONDITIONAL USE PERMIT
APPLICATION NO. 201601868
438 WEST TENNYSON ROAD
FINDINGS FOR APPROVAL**

Proposed Establishment of a Car Wash and Expanded Convenience Store at an Existing Gas Station at 438 West Tennyson Road (Assessor Parcel No. 465-0001-002-00 from Kevin Chiang, KC Associates/Xu Ying and Fan Li (Applicant/Owners) Requiring Approval of Conditional Use Permit Application No. 201601868.

California Environmental Quality Act.

- A. The project is categorically exempt from environmental review in accordance with the California Environmental Quality Act (CEQA) Guidelines, Section 15332, Infill Development Projects, in that the proposed project is consistent with applicable general plan policies and zoning designation and requirements; is located on an half-acre site that is surrounded by development on a site that is current being used as a gas station; is adequately served by utilities and public services; and, has no value as habitat for endangered, rare or threatened species. Redevelopment of the site to expand the convenience store and to add a car wash will not result in any significant effects related to traffic, noise, air quality or water quality.

Conditional Use Permit. Pursuant to Hayward Municipal Code (HMC) Section 10-1.3225, the Planning Commission may approve or conditionally approve a Conditional Use Permit application subject to the following findings:

- A. **The proposed use is desirable for the public convenience or welfare.**

The proposed project is desirable for the public convenience and welfare in that service stations, convenience stores and car washes are utilized by the general public on a regular basis; and, in that the proposed project will result in redevelopment of a generally underutilized and unattractive site with a relocated and expanded convenience store with restrooms; a self-service car wash and covered vacuum stations; increased site landscaping and tapered driveways; a more attractive fence and landscaped boundary along the shared property line with Tennyson Park; and more efficient and organized site circulation and parking.

- B. **The proposed use will not impair the character and integrity of the zoning district and surrounding area.**

The expanded convenience store and car wash will not impair the character and integrity of the CN (Neighborhood Commercial) District or surrounding area in that service stations and car washes are conditionally permitted uses; and, if the eastern

façade of the convenience store and the southern façade of the car wash structure are redesigned match the colors and materials utilized on other elevations, and are further articulated with windows, trim, awnings, mural, mosaic or other artistic element to provide visual interest from adjacent properties.

C. **The proposed use will not be detrimental to the public health, safety, or general welfare.**

The proposed project would not be detrimental to the public health, safety or general welfare in that the car wash would be enclosed within a concrete masonry structure and shielded from the neighboring properties by the trash enclosure and convenience store structures. If noise from the car wash exceeds allowable noise levels, conditions of approval would require installation of noise suppression equipment or automatic operating doors to further minimize noise. In addition, the site would have parking, vacuum stations and queuing to accommodate the service station and car wash uses.

D. **The proposed use is in harmony with applicable City policies and the intent and purpose of the zoning district involved.**

The proposed use is in harmony with applicable City policies and the intent and purpose of the CN District in that service stations and car washes are conditionally permitted uses; and, in that such uses are considered allowable supporting uses within the applicable Medium Density Residential General Plan land use designation. The proposed development will be consistent with the City of Hayward Design Guidelines related to avoidance of extensive blank walls if the eastern façade of the convenience store and the southern façade of the car wash structure are redesigned match the colors and materials utilized on other elevations, and are further articulated with windows, trim, awnings, mural, mosaic or other artistic element to provide visual interest from adjacent properties.

**CITY OF HAYWARD
PLANNING COMMISSION
CONDITIONAL USE PERMIT
APPLICATION NO. 201601868
438 WEST TENNYSON ROAD
CONDITIONS OF APPROVAL**

GENERAL

1. The permittee shall assume the defense of and shall pay on behalf of and hold harmless the City, its officers, employees, volunteers and agents from and against any or all loss, liability, expense, claim costs, suits and damages of every kind, nature and description directly or indirectly arising from the performance and action of this permit.
2. Conditional Use Permit Application No. 201601868, is approved subject to the Architectural Plans included as Attachment IV to the staff report, except as modified by the conditions listed below.
3. Applicant shall apply for all necessary building permits and/or all other related permits from the Building Division. All structures shall be constructed and installed in accordance with the California Building Code, Uniform Mechanical and Plumbing Code, National Electrical Code, and the California Fire Code as adopted by the City of Hayward.
4. All signage shall comply with the City of Hayward Sign Regulations. Sign permits shall be obtained prior to the installation of any new or relocated signage.
5. The owner shall maintain in good repair all building exteriors, walls, lighting, drainage facilities, landscaping, driveways, and parking areas. The premises shall be kept clean and weed-free.
6. The applicant shall be responsible for graffiti-free maintenance of the property, and shall remove any graffiti within 48 hours of occurrence or City notification.
7. If determined to be necessary for the protection of the public peace, safety and general welfare, the City of Hayward may impose additional conditions or restrictions on this permit.
8. The applicant, property owner, or designated representative, shall allow code enforcement staff access to the property for site inspections to confirm all approved use conditions have been completed and are being maintained in compliance with all adopted city, state, and federal laws.

9. The Planning Director may revoke this permit for failure to comply with, or complete all, conditions of approval or improvements indicated on the approved plans.
10. All outstanding fees owed to the City, including staff time spent processing this application, shall be paid in full prior to issuance of a building permit or certificate of occupancy.
11. This approval is void three (3) years after the effective date of approval unless a building permit application has been submitted and accepted for processing by the Building Official and substantial improvements have been made as determined by the Planning Director.
12. Any modification to this permit shall require review and approval by the Planning Director and/or the Planning Commission, if necessary.
13. Tobacco retail sales may continue with the proposed redevelopment of the site if the use is not discontinued for a period of six month or longer. If the use is discontinued for a period of six months or longer, or if the applicant fails to comply with all local, state, or federal laws applicable to tobacco products, electronic smoking devices or tobacco paraphernalia, and all requirements and operational standards contained within the City's Ordinance then tobacco sales shall be terminated in accordance with the Ordinance.
14. Violations of any approved land use conditions or requirements will result in further enforcement actions by the Code Enforcement Division. Enforcement includes, but is not limited to, fines, fees/penalties, special assessment, liens, or any other legal remedy required to achieve compliance including instituting a revocation hearing before the Planning Commission.
15. Failure to comply with any of the conditions set forth in this approval, or as subsequently amended in writing by the City, may result in failure to obtain a building final and/ or a Certificate of Occupancy until full compliance is reached. The City's requirement for full compliance may require minor corrections and/ or complete demolition of a non-compliant improvement regardless of costs incurred where the project does not comply with design requirements and approvals that the applicant agreed to when permits were pulled to construct the project.

PLANNING

16. A copy of these conditions of approval shall be included on a full-size sheet in the building permit plan set.
17. Loitering on the site is prohibited.

18. The hours of operation for the car wash shall be 7 a.m. to 8 p.m.
19. If noise from the carwash exceeds 70 dBA, measured at any property plane, the applicant shall install noise suppression equipment or install automatic doors at the entrance/exit of the car wash.
20. Modify the eastern elevation of the new convenience store to match the colors and materials and articulation used on other elevations such as windows or faux windows, trim and awnings, a belly-band between the first and second stories, a mural, mosaic or other artistic elements. The final design of the eastern elevation of the convenience store shall be provided on the building permit plans and shall be reviewed and approved by the Planning Division.
21. Modify the southern elevation of the car wash structure to match the colors and materials and articulation utilized on other elevation such as windows or faux windows, trim and awnings, a mural, mosaic or other artistic elements. The final design of the southern elevation of the car wash structure shall be provided on the building permit plans and shall be reviewed and approved by the Planning Division.
22. A decorative wrought iron metal fence shall be installed along the southern property line between the project site and Tennyson Park. The openings between the vertical and/or horizontal pickets should be less than three and one-half inches. The final fence design shall be reviewed and approved by the Planning Division prior to the issuance of building permits.
23. A minimum of three trash receptacles, with self-closing metal lids, shall be located near the entrance to the convenience store and near the vacuum parking areas. The final locations shall be shown on the building permit submittals and approved by the Planning Division prior to issuance of the building permit.
24. Outdoor storage is prohibited.
25. All rooftop equipment shall be shown on building permit submittals and adequately screened from view.
26. Utility meters, when not enclosed in a cabinet, shall be screened by either plant materials or decorative screen, allowing sufficient access for reading.

Building Division

27. At least one accessible route shall connect each story in multi-story buildings and facilities. If offices and bathing facilities are provided on a level not served by an elevator, then office and bathing facilities must be provided on the accessible ground floor. Some exceptions may apply (CBC 11B-206.2.3). Show details related to compliance on building permit submittal.

28. Car and van parking spaces shall be 216 inches (18 feet) long minimum (CBC 11B-502.2). Show details related to compliance on building permit submittal.
29. Access aisles shall extend the full required length of the parking spaces they serve (CBC 11B-502.3.2). Show details related to compliance on building permit submittal.
30. At least one of the parking stalls at the vacuum station must be accessible. Show details related to compliance on building permit submittal.
31. For clarity, provide building area analysis to show that the project complies within the allowable building heights and areas on plans.
32. Life and safety items such as fire-ratings of walls, opening protection (if any), egress, etc. shall be addressed on building permit submittal.
33. Eave overhangs and similar projections are not permitted to extend beyond the exterior wall if the fire separation distance (FSD) is two feet or less from the property line. Show details related to compliance on building permit submittal.
34. Remove the detectable warnings in the access aisles. Changes in level are not permitted in the access aisles (CBC 11B-502.4). Show details related to compliance on building permit submittal.
35. Provide the minimum plumbing fixtures analysis on building permit submittal.
36. Provide other building code requirement compliance details such as structural calculations, soils report, disabled access, plumbing, mechanical, electrical, T24 energy compliance, CALGreen compliance and others shall be reviewed at the building permit review process. Additional comments may apply depending on information provided in the building permit submittal.
37. The buildings shall be solar ready and comply with the requirements of CA Energy Code Section 110.10(b) through 110.10(d). Show details related to compliance on building permit submittal.
38. Provide capability to facilitate future installation of electric vehicle charging in the parking spaces. Provide and verify the required number of EV charging spaces. At least one of the spaces must be accessible. Show details related to compliance on building permit submittal.
39. Fuel-efficient vehicle parking and bicycle parking and locker may be required. Provide and verify the required number of fuel-efficient and car/van pool vehicle parking and short-term and long-term bicycle parking. Show details related to compliance on building permit submittal.

40. Supplemental building construction and improvement tax shall be required at the time of issuance of building permit.
41. School fee payment is required prior to issuance of building permit.

Development Review Services Engineering/Public Works Engineering

42. *Grading, Drainage, and Improvements:* A grading permit issued by the Public Works Department is required prior to the issuance of building permits. The grading permit application shall include engineered grading and drainage plans showing finish grades and site drainage along with improvement plans (both on-site and off-site) as part of the grading permit application. The grading and drainage plan shall include, but not be limited to, the following design & submittal requirements:
 - a) All on-site storm drainage conveyance facilities and earth retaining structures 4-foot in height or less (top of wall to bottom of footing) shall be reviewed and approved by Public Works. Earth retaining structures greater than 4-feet in height shall be reviewed and approved by the Building Division of the Development Services Department. The plans should include all proposed underground pipes, building drains, area drains and inlets. The on-site storm drainage system (if applicable) shall be designed to convey a 10-year storm event.
 - b) On-site and street lighting fronting the development shall comply with the City's Security Ordinance. Lighting shall be designed by a qualified lighting designer and erected and maintained so that light is confined to the property and will not cast a direct light or glare upon adjacent properties or rights-of-way.
43. *Stormwater Runoff Pollution Control Measures:* This project shall comply with the Alameda Countywide Cleanwater Program (ACCP) which requires the implementation of Best Management Practices (BMPs) which includes site design measures, source controls and numerically-sized Low Impact Development (LID) stormwater treatment measures to minimize stormwater pollutant discharges from the project site.
 - i. The project's Stormwater Control Plan and numeric sizing calculations have been reviewed and this project will be in conformance the ACCP.
 - ii. The property owner shall enter into the City's standard "Stormwater Treatment Measures Maintenance Agreement" as prepared by the City. The Maintenance Agreement shall be recorded with the Alameda County Recorder's Office to ensure that the maintenance responsibility for private treatment control and site design measures is bound to the property in perpetuity.

44. *Construction Damages*: The Developer shall be responsible to remove and replace curb, gutter, and sidewalk damaged during construction of the proposed project prior to issuance of the Final Construction Report by the City Engineer.

Landscaping

Landscaping and Irrigation Plans.

45. Detailed landscape and irrigation plans that are in full compliance with the City's *Bay-Friendly Water Efficient Landscape Ordinance, Tree Preservation Ordinance, Off-Street Parking regulations, and other applicable sections in Municipal Codes* shall be submitted and approved prior to the issuance of building permits. Plans shall be prepared by a licensed landscape architect, and minimum one set of plans shall be wet-stamped and wet-signed. In addition to the plans, a copy of the arborist report including appraised value of trees shall be submitted as a part of the building permit submittal package.
46. Tree mitigation for trees to be removed shall be provided above and beyond required trees. Required trees are in setback areas, parking lot shade trees and buffer trees. Plant a minimum of one twenty-four-inch-box tree at every twenty to forty feet on center depending upon tree species in the front yard setback areas and one fifteen-gallon evergreen screening tree at every twenty feet on center in the rear yard setback areas abutting Tennyson Park. The proposed Lagerstroemia is a deciduous tree therefore you must propose an evergreen tree.
47. Correct spelling in the tree mitigation summary chart prior to submittal of landscaping plans.
48. The plan base information shall include all above and underground utilities as well as bio-treatment areas. The architectural, site and civil plan plans shall be internally consistent.
49. Landscaping plans shall include the statement, "I have complied with the criteria of City of Hayward Bay-Friendly Water Efficient Landscape Ordinance and applied them for the efficient use of water in the landscape and irrigation design plan."
50. Landscape plans shall show plant spacing that is no less than the minimum spread at maturity. Proposed spacing for ground covers shall be revised and included on landscape plans.
51. Provide planting details and specifications on the landscape plans. Tree planting and staking detail shall conform to the City Standard detail SD-122 or equal except for palm planting. If SD-122 would be used, the detail shall be provided in the planting detail sheet.

52. A qualified soil testing laboratory shall complete a soil fertility test prior to amending the planting soil and shall provide recommendations for organic compost. The final testing results, recommendations and organic compost shall be submitted to City Landscape Architect prior to requesting the final landscape inspection with Appendix C Certification of Completion.
53. Root barriers shall be installed linearly against the paving edge in all instances where a tree is planted within seven feet of pavement or buildings, and as directed by the landscape architect.
54. Plant fertilizers shall be Organic Materials Review Institute (OMRI)-certified.
55. Detailed irrigation plan, specifications and details shall be submitted and approved prior to the issuance of building permits.
56. Irrigation plans shall show the dedicated irrigation meter size as well as the size and location of backflow prevention device. Backflow prevention device shall conform to the City Standard Detail SD-202 and the detail shall be provided on a detail sheet.
57. Bio-treatment areas that are wider than ten feet shall be irrigated with matched precipitation rotator type, or as efficient overhead spray irrigation system allowing “cycle and soaking” program function on a separate valve.
58. Prior to the issuance of Certificate of Occupancy, all landscape and irrigation shall be completed in accordance to the approved plan and accepted by the City Landscape Architect. Before requesting an inspection from the City Landscape Architect, the project landscape architect shall inspect and accept landscape improvements and shall complete Appendix C. Certificate of Completion in the City’s Bay-Friendly Water Efficient Landscape Ordinance. The completed Certificate of Completion Part 1 through Part 7 or applicable parts shall be faxed/e-mailed/turn in prior to requesting an inspection from the City Landscape Architect.

Tree Preservation.

59. A tree preservation bond will be required for all trees that are to remain, and the bond will be in effect throughout the construction period and until completion of the entire project improvements. If any trees that are designated as saved are removed or damaged during construction shall be replaced with trees of equal size and equal value.
60. Trees shall be preserved in accordance with the *Tree Preservation Ordinance*. Prior to the commencement of clearing and grading operations, all trees to be preserved or removed shall be indicated on the grading, site and landscape plans, and trees to remain in place shall be noted and provided with tree protection measures in compliance with City codes.

61. A tree removal permit shall be obtained prior to the removal of any tree in addition to demolition permit or grading permit.

Landscape Maintenance.

62. Landscaping shall be maintained in a healthy, weed-free condition at all times and the irrigation system shall reduce runoff, promote surface filtration, and minimize the use of fertilizers and pesticides, which contribute pollution to the Bay.
63. The owner's representative shall inspect the landscaping on a monthly basis and replace dead or dying plants (plants that exhibit over 30% dieback) within ten days of the inspection.
64. Three inches deep mulch should be maintained in all planting areas. Mulch shall be organic recycled chipped wood in the shades of Dark Brown Color.
65. All nursery stakes shall be removed during tree installation and staking poles shall be removed when the tree is established or when the trunk diameter of the tree is equal or larger to the diameter of the staking pole.
66. All trees planted as a part of the development as shown on the approved landscape plans shall be "Protected" and shall be subjected to Tree Preservation Ordinance.
67. Tree removal and pruning shall require a tree pruning or removal permit prior to removal by City Landscape Architect. Any damaged or removed trees without a permit shall be replaced in accordance with Tree Preservation Ordinance or as determined by City Landscape Architect within the timeframe established by the City and pursuant to the Municipal Code.
68. The irrigation system shall be tested periodically to maintain uniform distribution of irrigation water. The irrigation controller shall be programmed seasonally and shall be shut-off during winter season, and shall be flushed and cleaned in the spring.

Utilities and Environmental Services

Water:

69. The property is served by a 5/8-inch domestic water meter (Account # 21722). If the existing water service and meter cannot be reused, it must be abandoned by City Water Distribution Personnel at the applicant's/owner's expense. Plans indicate that the existing 5/8-inch domestic water meter will be reused as an irrigation meter. The gallon-per-minute demand for the irrigation system must be provided to determine proper meter size.

- a. The gallon-per-minute demand for the irrigation system shall be provided to determine proper meter size. Plans for the building permit must show the gallon-per-minute demand of the irrigation system.
 - b. The civil plans indicate that there will be two Reduced Pressure (RP) backflow prevention assemblies installed for the irrigation water meter. On the civil and landscaping plans, revise the two RP backflow prevention assemblies to show one RP backflow prevention assembly.
70. Plans indicate that a new domestic water meter will be installed to serve the mini-mart building. Plans must be revised to show the size of the new domestic water meter, service line, and RP backflow prevention assembly. Based on the water fixtures show on the plans for the mini-mart building, the finished structure will have approximately 21.5 water fixture units, which requires a minimum 5/8" domestic meter. The cost for a new 3/4-inch service line and 5/8-inch domestic meter is \$9,984 (\$3,500 installation fee + \$6,484 facilities fee).
 - a. Building permit submittal plans shall indicate the size of the domestic water meter, service line, and the RP backflow prevention assembly that will be installed for the mini-mart.
71. Plans indicate that a new one-inch domestic meter will be installed to serve the car wash. Based on Sheet C2.1, the car wash will have a demand of 38 gallons per minute, which requires a minimum one-inch domestic water meter. The cost for a new one-inch service line and one-inch domestic meter is \$19,710 (\$3,500 installation fee + \$16,210 facilities fee). The building permit submittal plans shall show the location and size of the new service line, meter, and the RP backflow prevention assembly.
72. The civil plans and landscaping plans provided show inconsistent water supply lines for irrigation and domestic use. On the plans for the building permit, revise plans to ensure that they are consistent.
73. New fire services, if required, must be installed by City Water Distribution Personnel at the owner's/applicant's expense. Fire service installations are billed on an actual cost basis with a time and materials deposit due prior to the start of installation. The final billing of the job will be the actual costs of the work performed and materials used. If actual costs are less than the deposit amount, the owner/applicant will receive a refund in the amount of the unused deposit. If actual costs exceed the deposit amount, the owner/applicant will receive an invoice in the amount of the overage.
74. All domestic & irrigation water meters must have Reduced Pressure (RP) Backflow Prevention Assemblies, per City of Hayward Standard Detail 202. Backflow Prevention Assemblies shall be at least the size of the water meter or the water line on the property side of the meter, whichever is larger.

75. Water meters and services to be located a minimum of two feet from top of driveway flare as per City Standard Details SD-213 thru SD-218.
76. Water mains and services, including the meters, must be located at least ten feet horizontally from and one-foot vertically above any parallel pipeline conveying untreated sewage (including sanitary sewer laterals), and at least four feet from and on foot vertically above any parallel pipeline conveying storm drainage, per the current California Waterworks Standards, Title 22, Chapter 16, Section 64572. Minimum horizontal separation distances may be reduced by using higher grade piping materials.
77. Per the City of Hayward Municipal Code, new commercial car washes must utilize recirculating water systems. Provide design detail showing compliance with this provision on the building permit submittal.

Sewer:

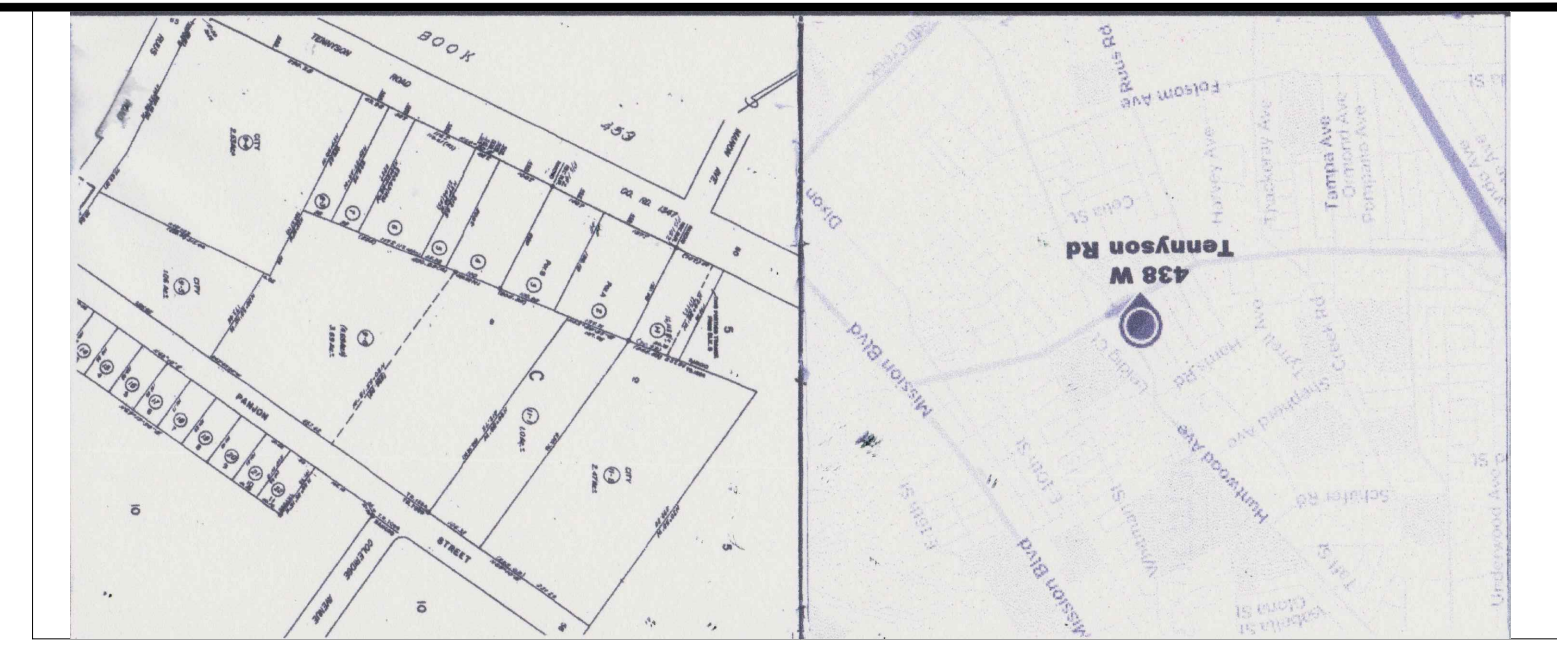
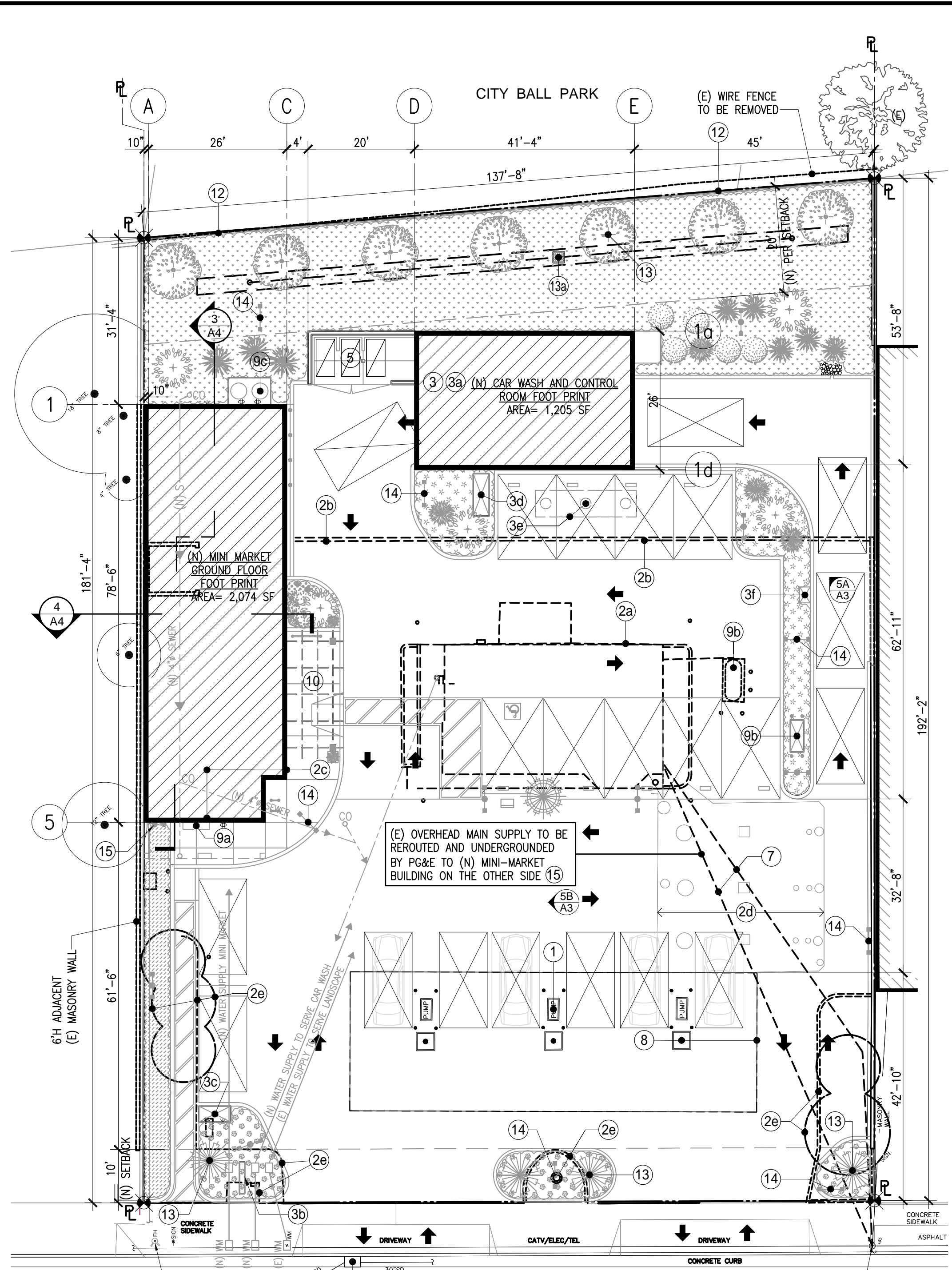
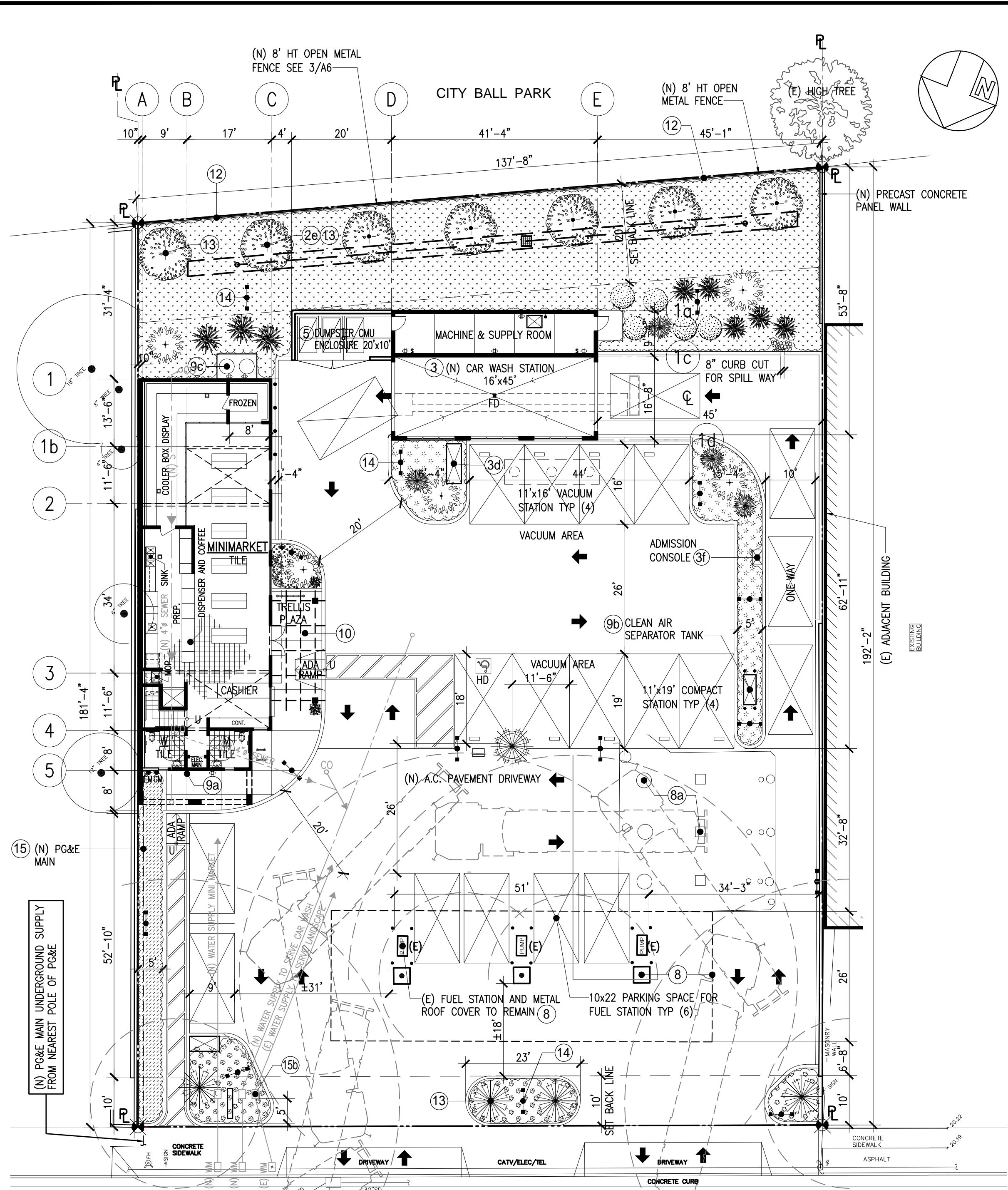
78. The sanitary sewer laterals shall have cleanouts and be constructed per City Standard Detail SD-312. Design details shall be provided on building permit submittal.
79. Sewer connection fees for non-residential connections are calculated based on the volume and strength of the wastewater discharge. The property has a permitted discharge capacity of 210 gallons per day of domestic strength discharge. Sewer System Connection/Capacity to accommodate the volume and waste strength of wastewater to be discharged from the subject project may need to be purchased, at the rates in effect at the time of purchase, prior to discharge. Based on the information provided on Civil Sheet C2.1, the discharge for the car wash has been calculated at 600 gallons per day. The cost for this additional wastewater discharge has been calculated at \$22,013. This fee will be due at the time the building permit is issued.

Solid Waste

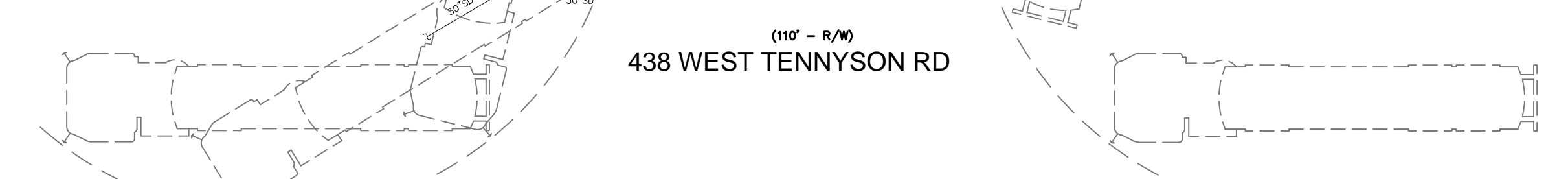
80. All businesses shall arrange for separate collection of recyclables and all businesses that generate significant organics must arrange for separate collection of organics (food, food-soiled paper, and plant debris). For more information, please visit <http://www.recyclingrulesac.org/city/city-of-hayward/>.
81. Construction & Demolition Debris: All construction and demolition debris shall be recycled per certain ordinance requirements. Submittal of the Debris Recycling Statement is required with building permit submittal. The form can be found at <http://www.hayward-ca.gov/services/city-services/construction-and-demolition-debris-disposal>.

Fire Department/Hazardous Materials

82. The Hayward Fire Department shall be immediately notified if ground water and/or soil contamination is discovered during demolition, and/or removal of any fueling systems, underground storage tanks, associated piping, etc. An approved and acceptable means shall be established to remediate the contamination.
83. Any modifications/upgrades of the existing underground fuel system, underground storage tanks, monitoring panel, associated piping, clean air separator, under dispenser pans, dispensers, sensors, alarms, overfill prevention system, or any other element of the fueling system will require a separate Fire Department permit and review.
84. Installation of the aboveground Propane retail tank exchange and tank storage will require a separate fire permit. The location of the propane tanks may need to be relocated from under the AC deck to another approved location.
85. Removal, relocation, reinstallation of the existing underground storage tank monitoring system panel shall only be done by continuous uninterrupted power source. The underground storage tanks shall be continuously monitored at all times throughout the duration of the project.
86. Construction of the new building and new fuel islands and dispensers should meet California Fire Code and California Building Code.
87. Install approved type portable fire extinguishers within the mini-mart and at the fuel island(s), as required by the 2016 California Fire Code (CFC).
88. No commercial cooking equipment allowed within the mini-mart unless reviewed and approved by the Hayward Fire Department.
89. Signage and labeling on fuel islands shall meet compliance from the California Fire Code (CFC) and local Ordinance.
90. Address numbers shall be six-inches minimum on a contrasting background to be visible from the street.
91. New fuel island canopy footing locations shall be approved before they are installed.



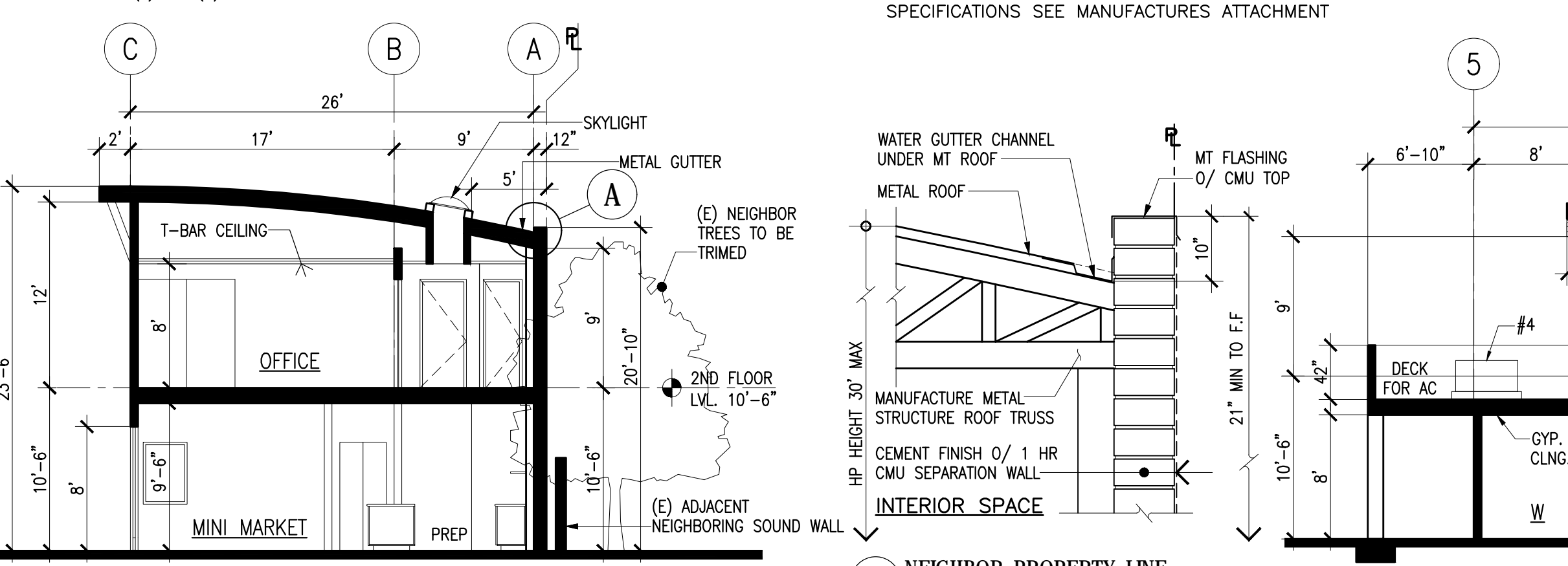
- THIS PERMIT APPLICATION IS FOR AN (E) GAS STATION EXPANSION, REFURBISHMENT AND UPGRADE UNDER CONDITIONAL USE PERMIT PROJECT AS FOLLOWS WHICH INCLUDE:
- EXISTING (3) FUEL STATIONS ALONG W/ (E) UNDERGROUND OIL STORAGE TANK REMAIN INTACT UNDER THE SAME (E) METAL ROOF CANOPY.
 - DEMOLISH (E) ONE STORY MINI-MARKET (±1,000 SF)
 - DEMOLISH (E) LOW CONCRETE BARRIER BLOCK
 - (N) MODERN STYLISH 2-STORY MINI-MARKET=3,062 SF, INCLUDING 2,074 SF RETAIL, MEN AND WOMEN ACCESSIBLE RESTROOMS AT GROUND, 988 SF OFFICE ON 2ND FLOOR.
 - (E) UNDERGROUND GASOLINE STORAGE TANK TO REMAIN W/ MONITOR SYSTEM TO BE REROUTED AND UPGRADED
 - (E) PLANTS, CURB, LIGHT POLE AND ABANDONED PUBLIC PHONE TO BE RELOCATED OR DEMOLISHED
 - (E) STREET CURB STORM CATCH BASIN
 - (E) STREET FIRE HYDRANT
 - (E) STREET PG&E POLE
 - PROVIDE ONE (N) CAR WASHING BY SHELL W/ AUTOMATIC WASHING EQUIPMENT INSIDE AND DIGITAL CONTROL CONSOLE OUTSIDE.
 - (N) CONTROL ROOM 363 SF
 - RELOCATED (E) MONUMENT SIGN W/ SEPARATE PERMIT APPLICATION
 - (N) TIRE PRESSURE STATION
 - (N) VACUUM PRESSURE BOOSTER
 - (N) CAR WASH UNDERGROUND RECYCLE WATER TANK
 - (N) CAR WASH CUSTOMER/ CONSOLE TICKET SERVICE STAND
 - PROVIDE 9 (N) VACUUM STATIONS OF SELF HELP AND SERVED FOR 24 HR A DAY, ONE IS FOR ACCESSIBLE ELECTRICAL VEHICLE STATION
 - (N) DUMPSTER 20'x10'x10'H ENCLOSURE FOR GARBAGE AND RECYCLED PURPOSE AT BACK YARD
 - PROVIDE (N) 2 PARKING STALLS FOR MINI-MARKET CLIENTS, ONE SPACE IS FOR AIR/ WATER SUPPLY AS SHOWN
 - (E) PG&E MAIN SUPPLY LINES ARE REROUTED AND UNDERGROUND TO (N) MINI-MARKET
 - (E) 3 FUEL STATIONS W/ (E) METAL COVER SHADE AND STRUCTURE COLUMNS TO REMAIN INTACT W/ (N) 42" H STEEL GUARD POSTS (4) AROUND EACH PUMP. 8a (E) GASOLINE AND DIESEL UNDERGROUND TANKS TO REMAIN INTACT
 - (N) RELOCATED PROPANE CAGE RACK
 - (E) CLEAN AIR SEPARATOR TANK TO BE RELOCATED
 - (N) AC COMPRESSOR
 - (N) WOOD TRELLIS W/ IVY ON TOP
 - (E) WATER MAIN AND SEWER LINES TO BE UPGRADED AND REROUTED TO SERVE (N) USES
 - (N) 8' HT METAL OPEN CAST IRON FENCE ON CONCRETE RETAINING CURB ALONG REAR YARD
 - (N) TREES AND PLANTS PER LANDSCAPE PLAN
 - BIO RETENTION
 - DOUBLE HEAD PARKING LIGHT POLES TOTAL (15) TYP. OR (E) TO BE REPLACED.
 - (N) PG&E MAIN SUPPLY LINES
 - (N) WATER/ AIR PRESSURE STATION
 - BACK-FLOW PREVENTOR BOX 5' AWAY FROM SIDEWALK W/ PROTECTION CAGE SEE CITY STANDARD DETAIL SD-201 OR SD-202
 - LANDSCAPE AREA= 5,553 SF
 - LOT SIZE= 25,628 SF
 - NEW 2-STORY MINI-MARKET:
 - GROUND= 2,074 SF
 - 2ND= 988 SF
 - SUBTOTAL= 3,062 SF
 - BUILDING COVERAGE= $B_0/A = 2,074 / 25,628 = 8\%$
 - LANDSCAPE AREA= 5,553 SF (SEE 10/A3 DIAGRAM)
 - IMPERVIOUS AREA= A-E= PARKING+ DRIVEWAY+ CONC SIDEWALK+ CAR-WASHING+ DUMPSTER ENCLOSURE+ PLAZA+ FUEL STATIONS= 25,628 SF - 5,553 SF= 20,075 SF
- CODES AND REGULATIONS COMPLIANCE
- | California Codes | City Ordinance # |
|--|------------------|
| a. California Fire Code 2016 Edition | #24-2016 |
| b. California Residential Code 2016 Edition | #23-2013 |
| c. California Building Code 2016 Edition | #23-2013 |
| d. California Mechanical Code 2016 Edition | #23-2016 |
| e. California Plumbing Code 2016 Edition | #23-2013 |
| f. California Electric Code 2016 Edition | #23-2013 |
| g. California Green Building Code 2016 Edition | #23-2013 |
| h. Wildland-Urban Interface Fire Area 2016 Edition | #23-2016 |
- G. OCCUPANCY USE GROUP= B (GAS STATION AND MINI MARKET) AND R-1 (2ND FLOOR FOR EMPLOYEE BOARDING)
- H. CONSTRUCTION TYPE VB (NO FIRE SPRINKLER REQUIRED), AND 1HR CMU SEPARATION WALL ALONG EAST ADJACENT PROPERTY
- I. PARKING SPACES TOTAL (21)
 HD= 1 W/ EV STATION PER SEPARATE PERMIT
 REGULAR= (2)
 FUEL STATION= (6)
 VACUUM STATION= (8)
 CAR WASH QUEUE LINE (4)



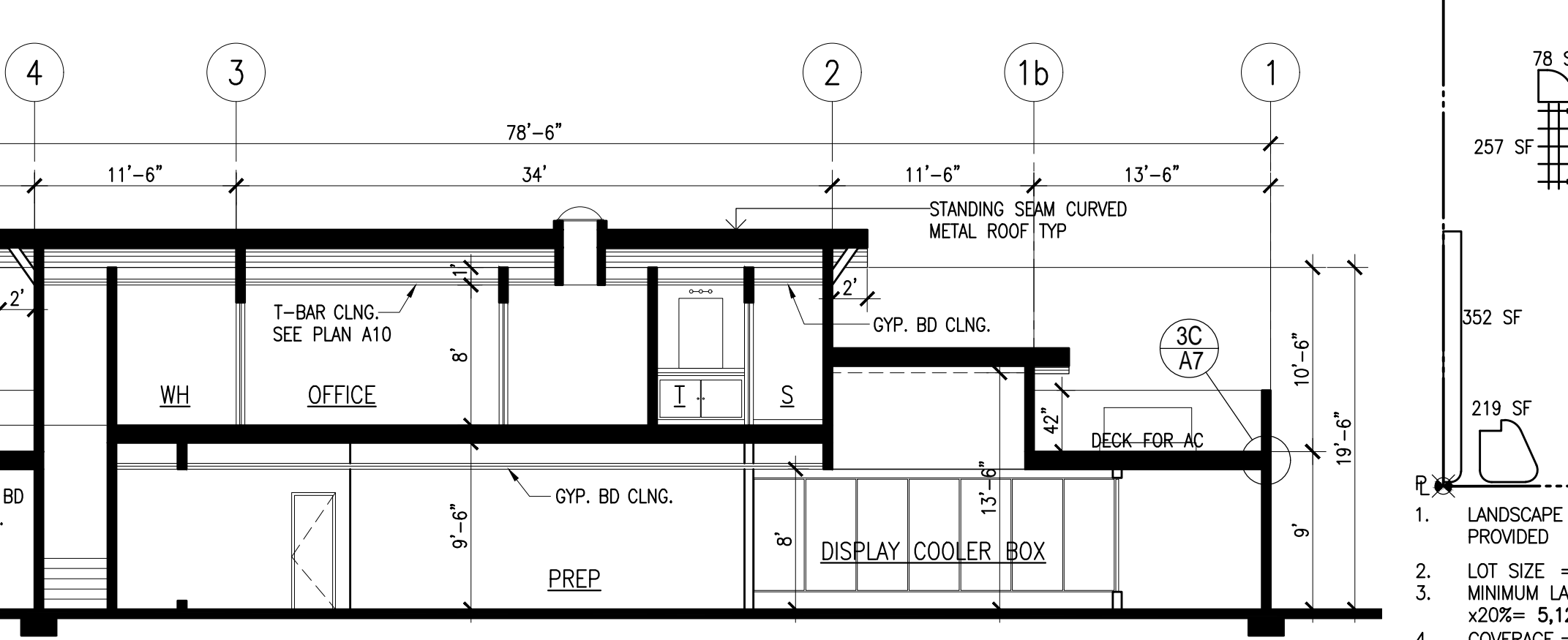
NOTE: 1 (N) 24" BOX STREET TREE OR (N) 15 GALLON BUFFER TREE W/ MINI 6'-6" SHADE-CLEARANCE HT. ALL TRESS ALONG DRIVEWAY AND PARKING ISLANDS SHALL BE MIN 6'HT CLEARANCE FOR CAR CLEARANCE
 2 PLUMBING CONTRACTOR SHALL CONDUCT FIELD VERIFICATION ON WATER SUPPLY CAPACITY OF (E) AND (N) WATER METERS TO SERVE VARIOUS FUNCTIONS PER DESIGN DESIGNATION



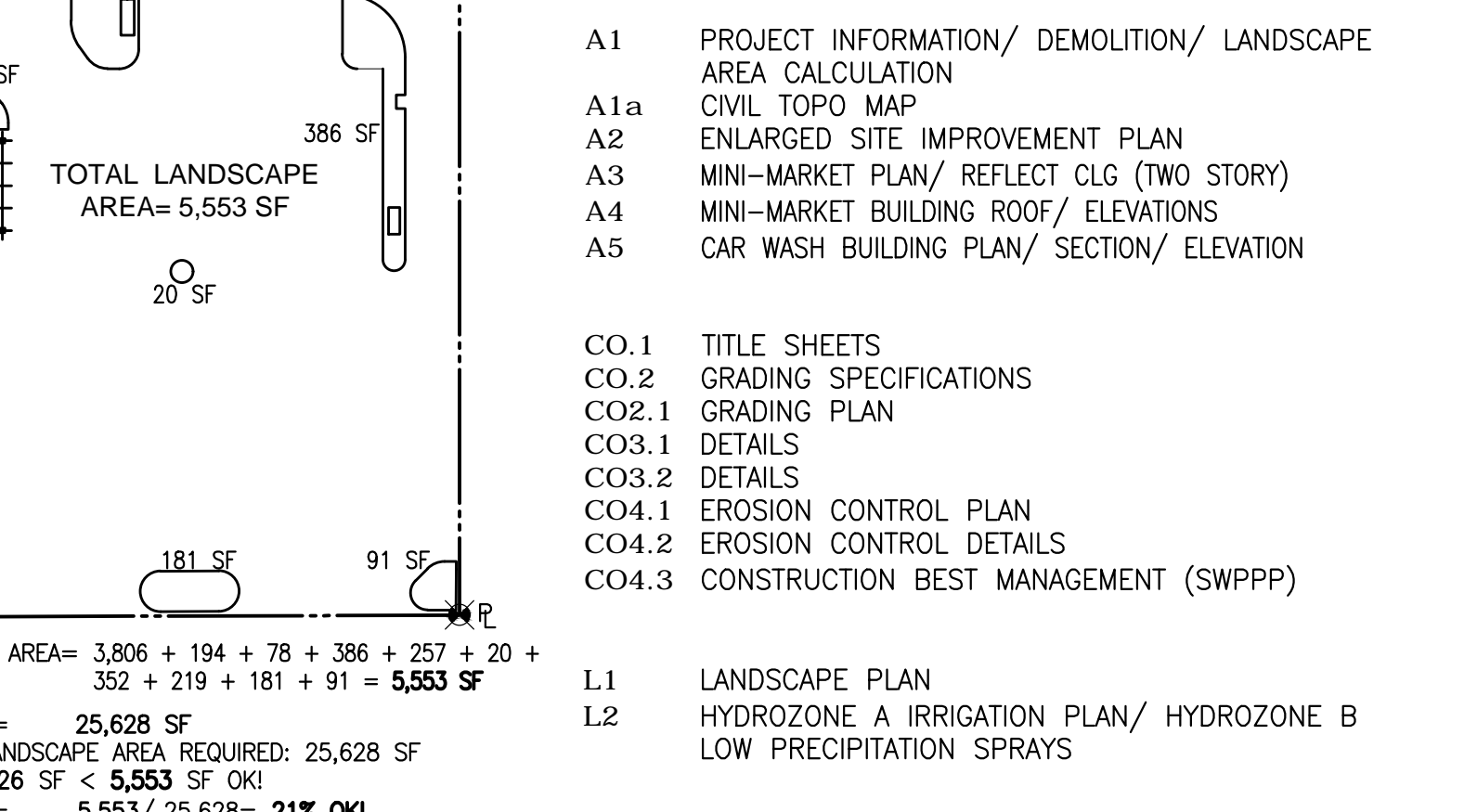
1 DEMOLITION AND (N) SITE PLAN OVERLAY 1/16"=1'
 SEE A1a FOR (E) CIVIL TOPO MAP ON ACCURATE EXISTING CONDITION
 SEE A3 FOR (E) PHOTOS ON CURRENT CONDITIONS
 (N) BUILDING STRUCTURE LOCATION FOR MINI-MARKET AND CAR WASH SHADE
 (E) BUILDING AND GROUND SURFACE STRUCTURE AND OVER HEAD PG&E LINES TO BE DEMOLISHED OR RELOCATED
 (N) PARKING SPACES AND ISLANDS TO BE INSTALLED AND (E) FUEL STATIONS W/STRUCTURE COVER SHADE REMAIN INTACT



4 SECTION A-A 1/8"=1'



3 SECTION B-B 1/8"=1'



3 LANDSCAPE AREA TOTAL= 5,553 SF

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LICENSED ARCHITECT
 No. 12317
 STATE OF CALIFORNIA

GAS STATION AND CAR WASH EXPANSION AND UPGRADE
 438 WEST TENNYSON RD

Sheet Title

Date 05-20-18

Scale AS SHOWN

Drawn

Job

File

Sheet A1

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GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD

Sheet Title

Date 05-20-18

Scale AS SHOWN

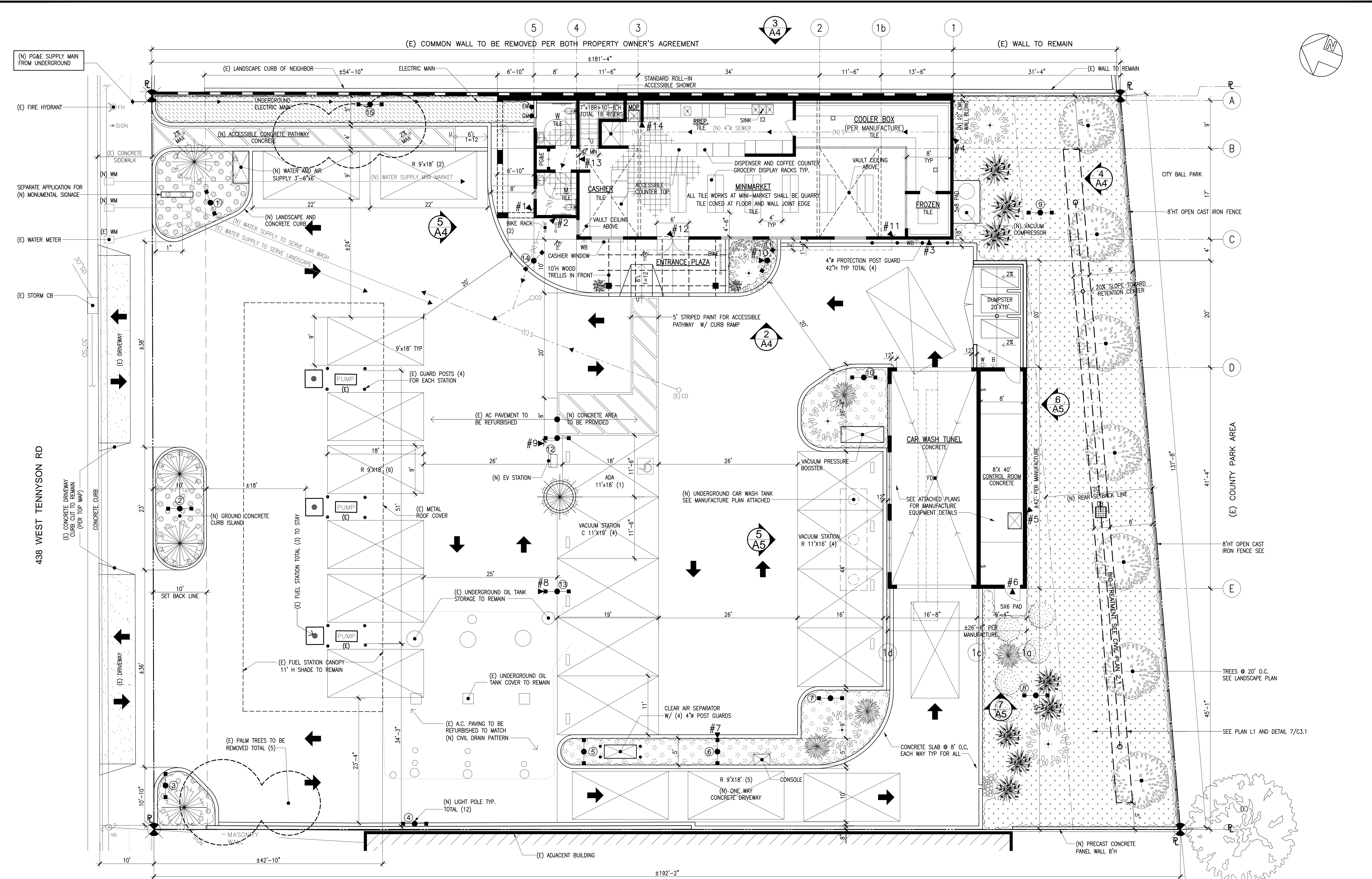
Drawn

Job

File

Sheet

A2



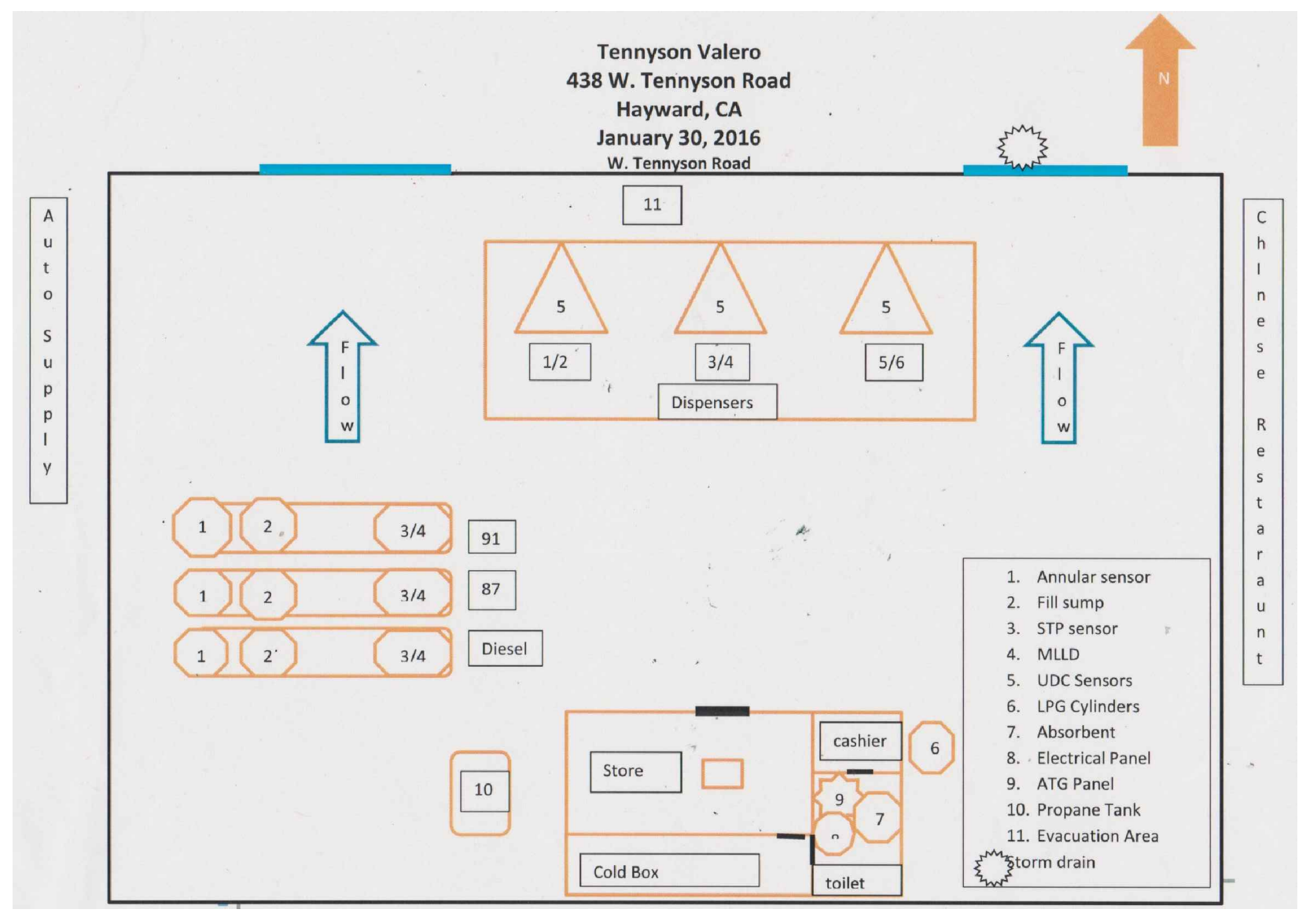
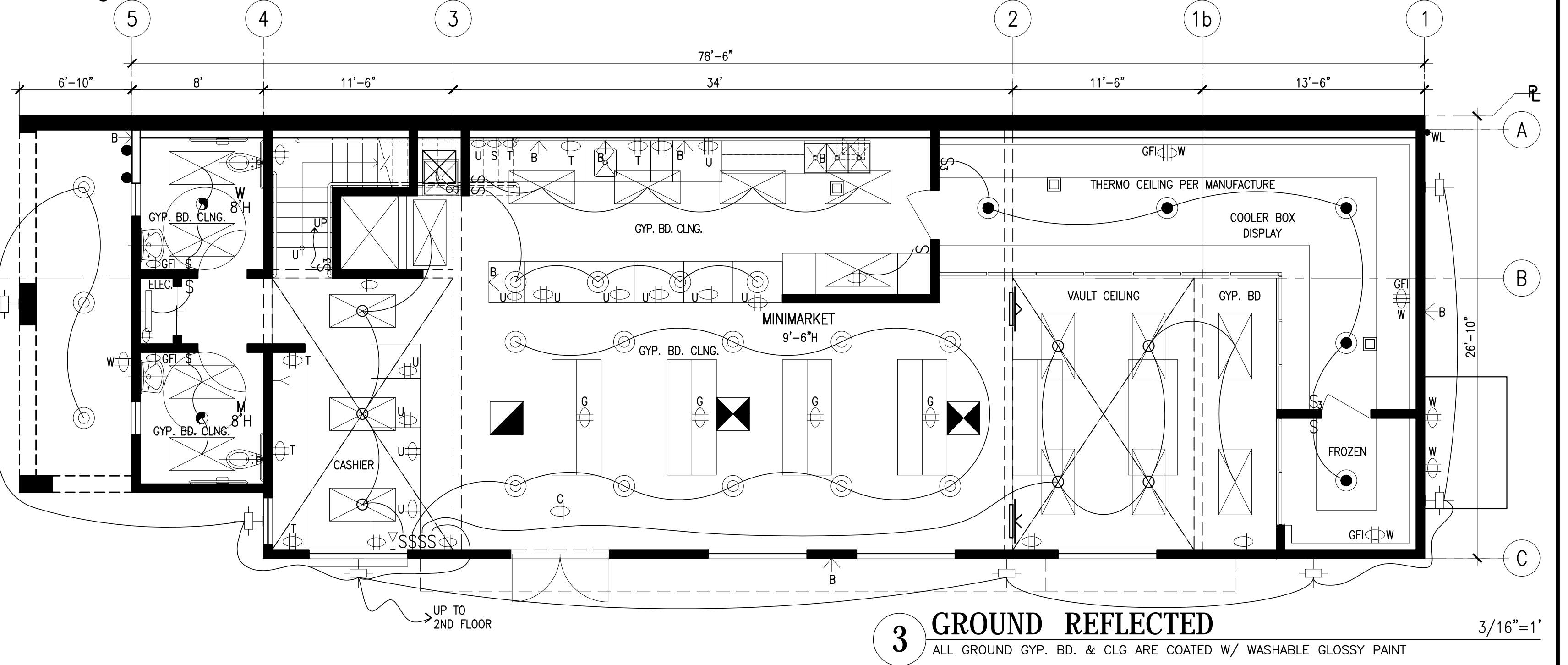
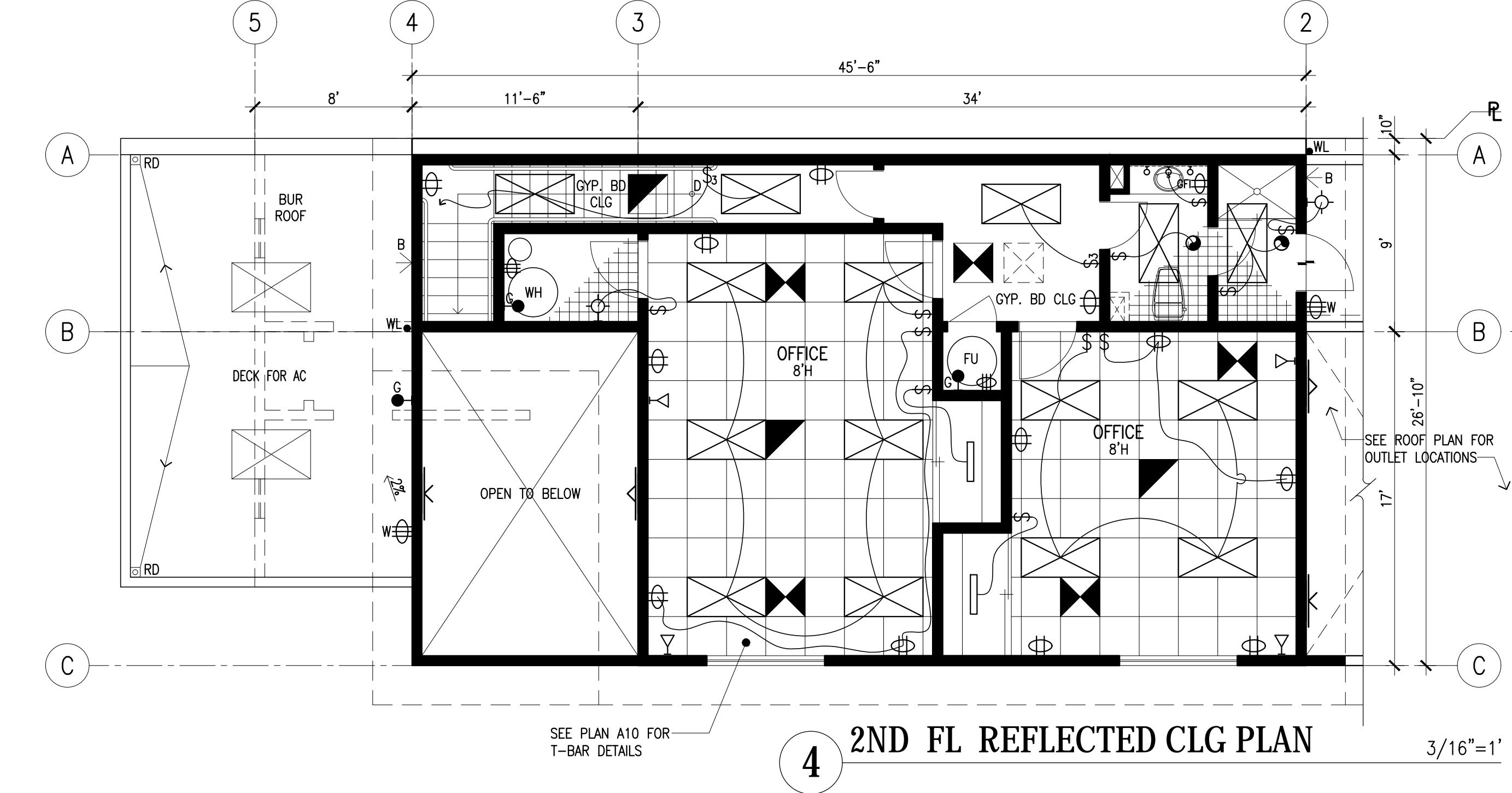
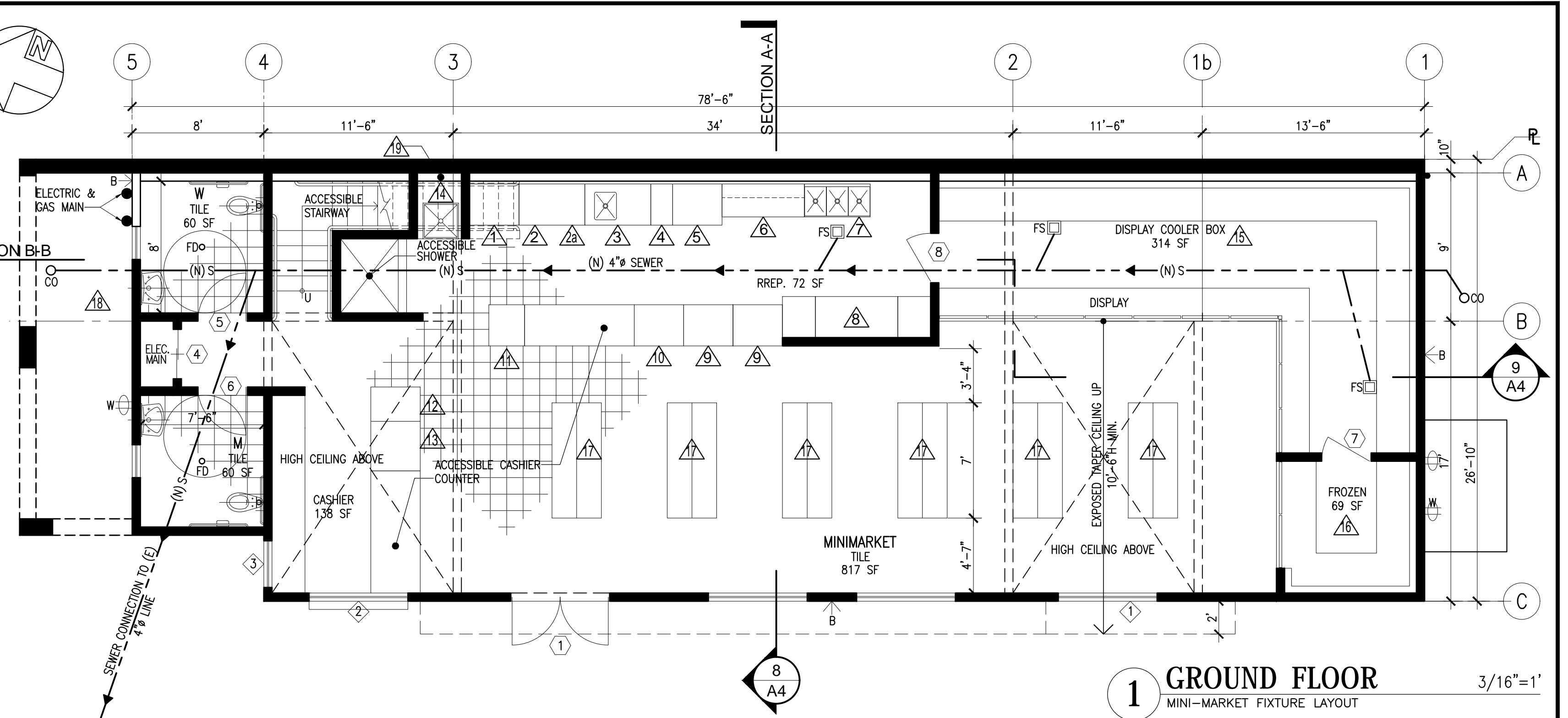
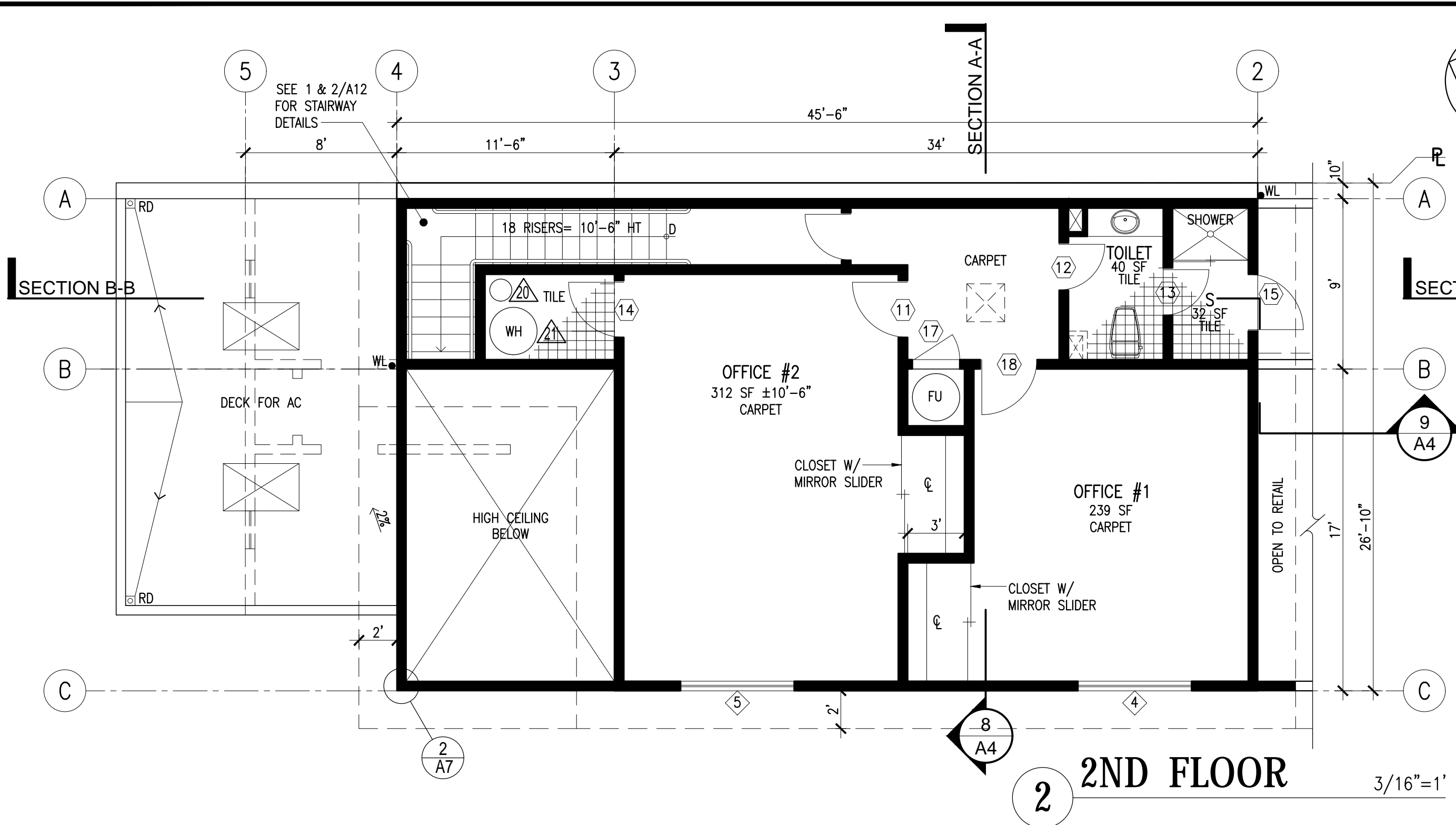
- 1. TOTAL LOT AREA= 25,628 SF
- 2. TOTAL LANDSCAPE AREA= 5,296 SF
- 3. BUILDING: a. MINI-MARKET= GROUND FLOOR 2,074 SF
+ 2ND FLOOR 988 SF= 3,062 SF
- b. CONTROL RM= 402 SF
- c. CAR WASH SHELL AREA= 803 SF
- d. TOTAL BUILDING SHELL FOOT PRINT AREA= 3,279 SF

- 4. BUILDING COVERAGE $3a+3c+3d/1 = 2,074+ 402+ 803/ 25,628= 13\%$
- 5. LANDSCAPE RATIO= $25,628 \times 20\% = 5,126$ SF REQUIRED AND (N) LANDSCAPE AREA 5,296 SF PLUS IVY COVER ON TOP OF ENTRANCE TRELLIS. OK!
- 6. PARKING SPACES PROVIDED= 21
REGULAR= 2
FUEL STATION= 6
VACUUM STATIONS= COMPACT 4 (11'x16')+ REGULAR 4 (11'x19')=8
CARWASH QUEUE LINE= 4
ADA= 1
- 7. TOTAL (N) LIGHT POLES (15) TYP INSIDE SITE

- LEGEND:
- #1 SECURITY CAMERA IN STORE AND OUTSIDE TOTAL (15)
 - ① 25' PARKING LIGHT POLES TOTAL (15)
 - (E) NEIGHBORING CONCRETE FENCE WALL TO BE REMOVED AND LEAVE COMMON NON-BARRIER OPEN SPACE IN FRONT PER AGREED BY BOTH PROPERTY OWNERS

1 (N) SITE PLAN
SEE TOPO MAP FOR EXISTING SITE CONDITION

1/8"=1'

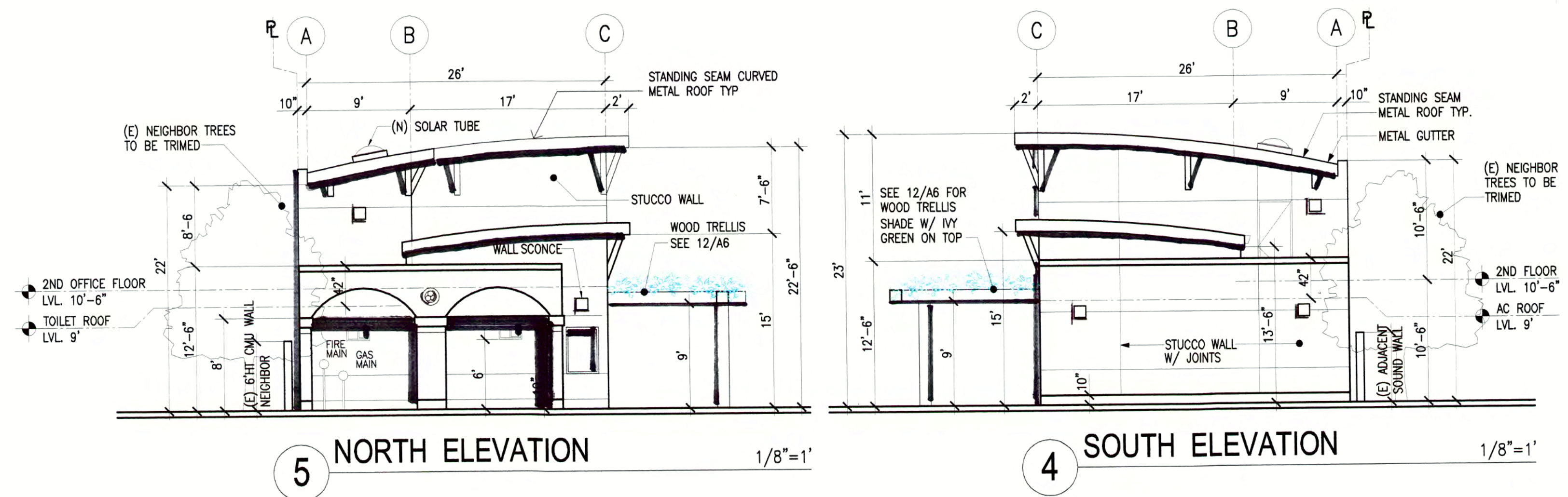
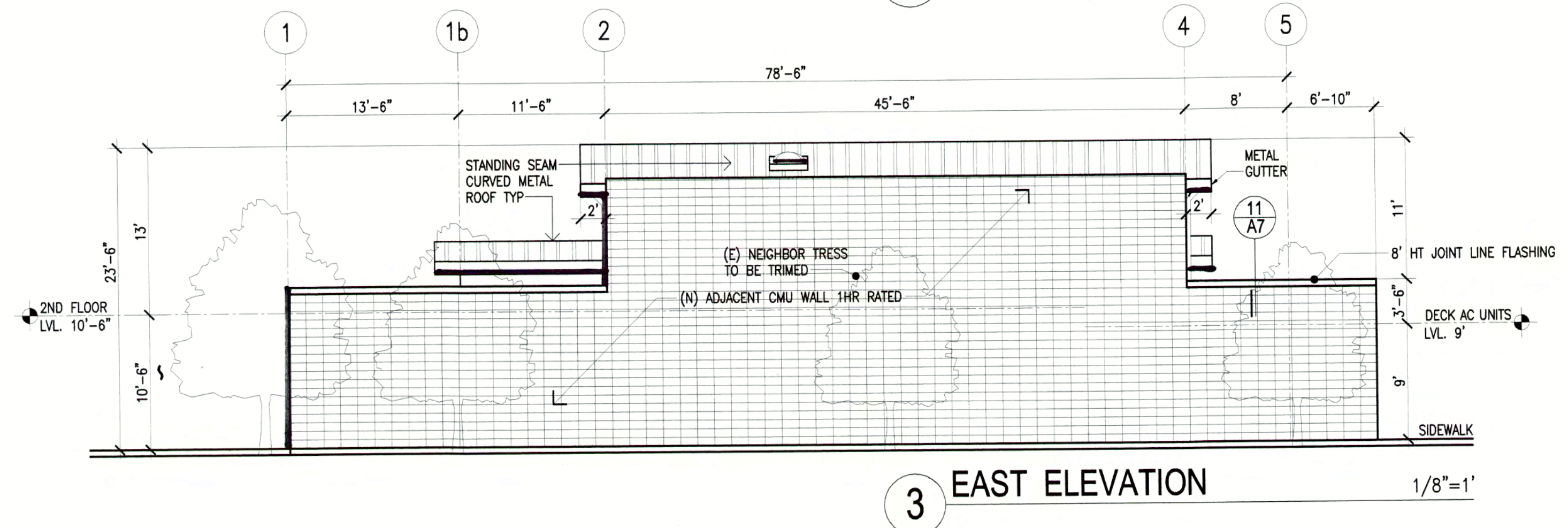
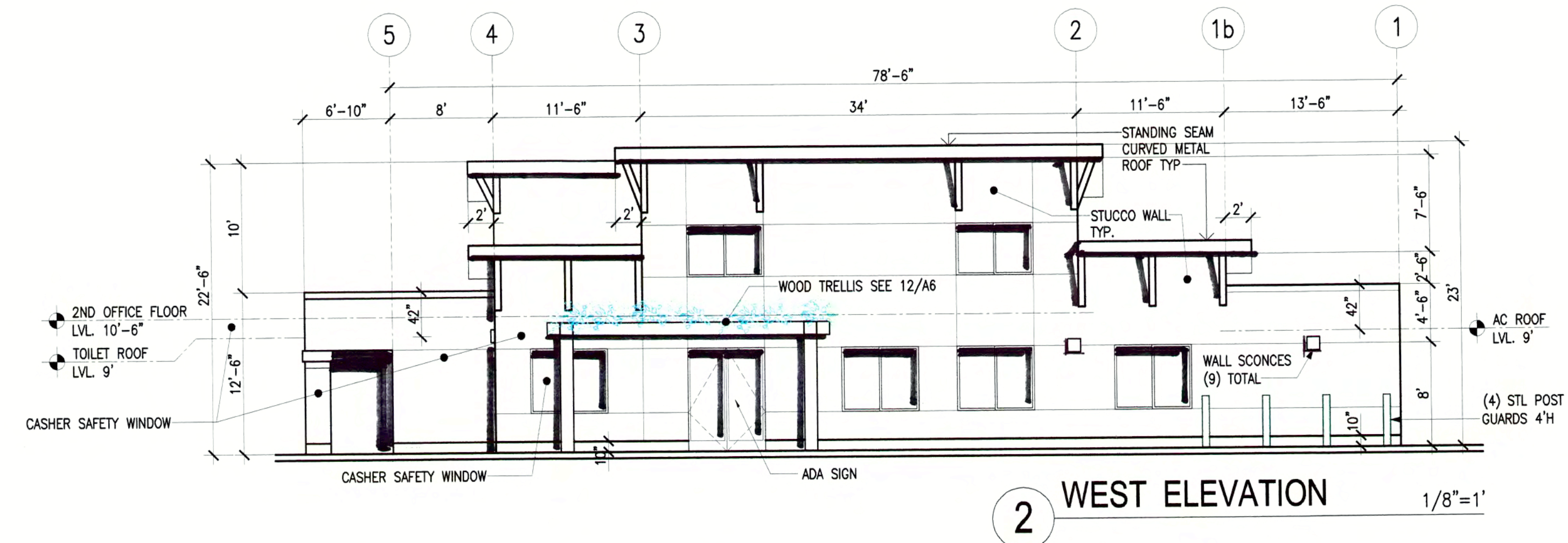
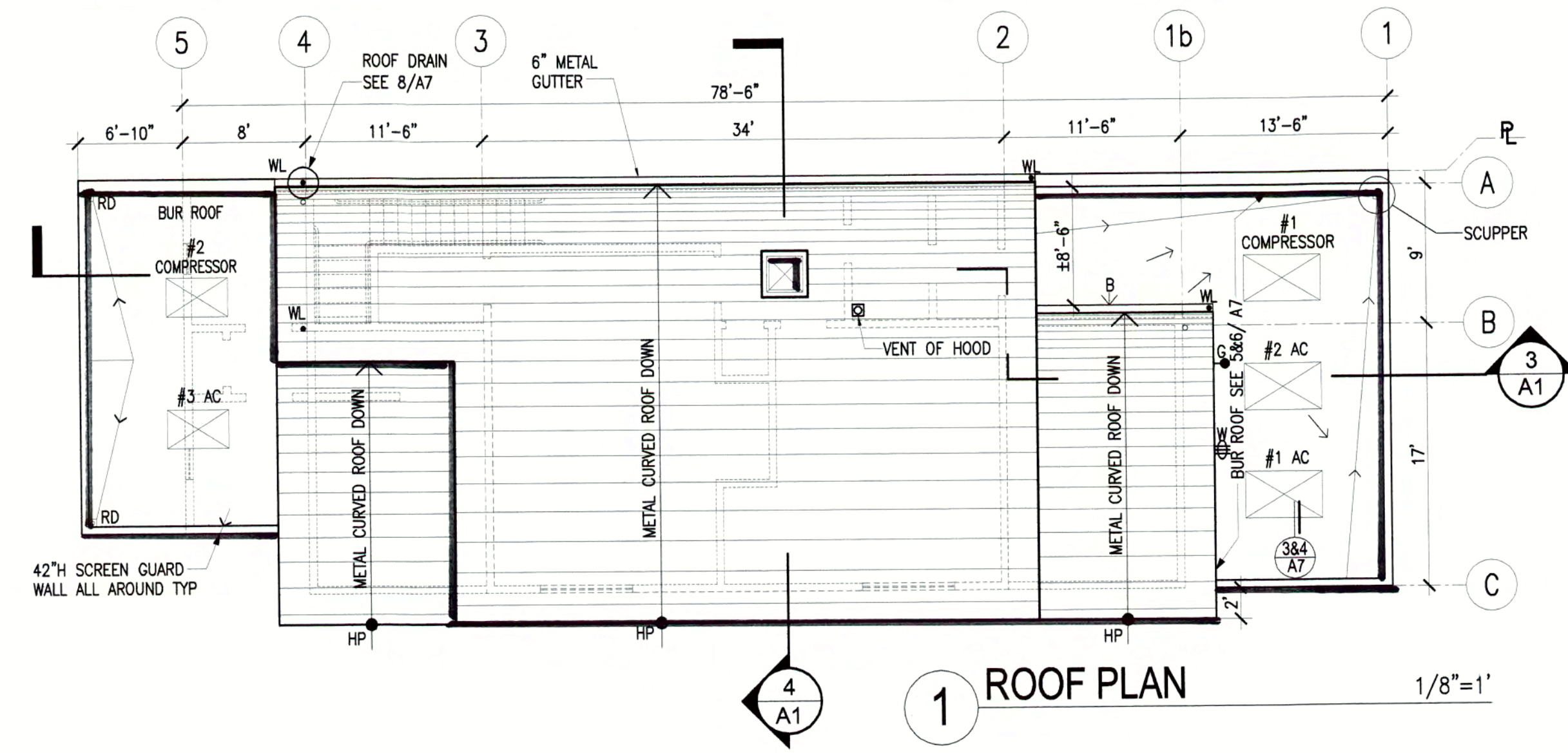
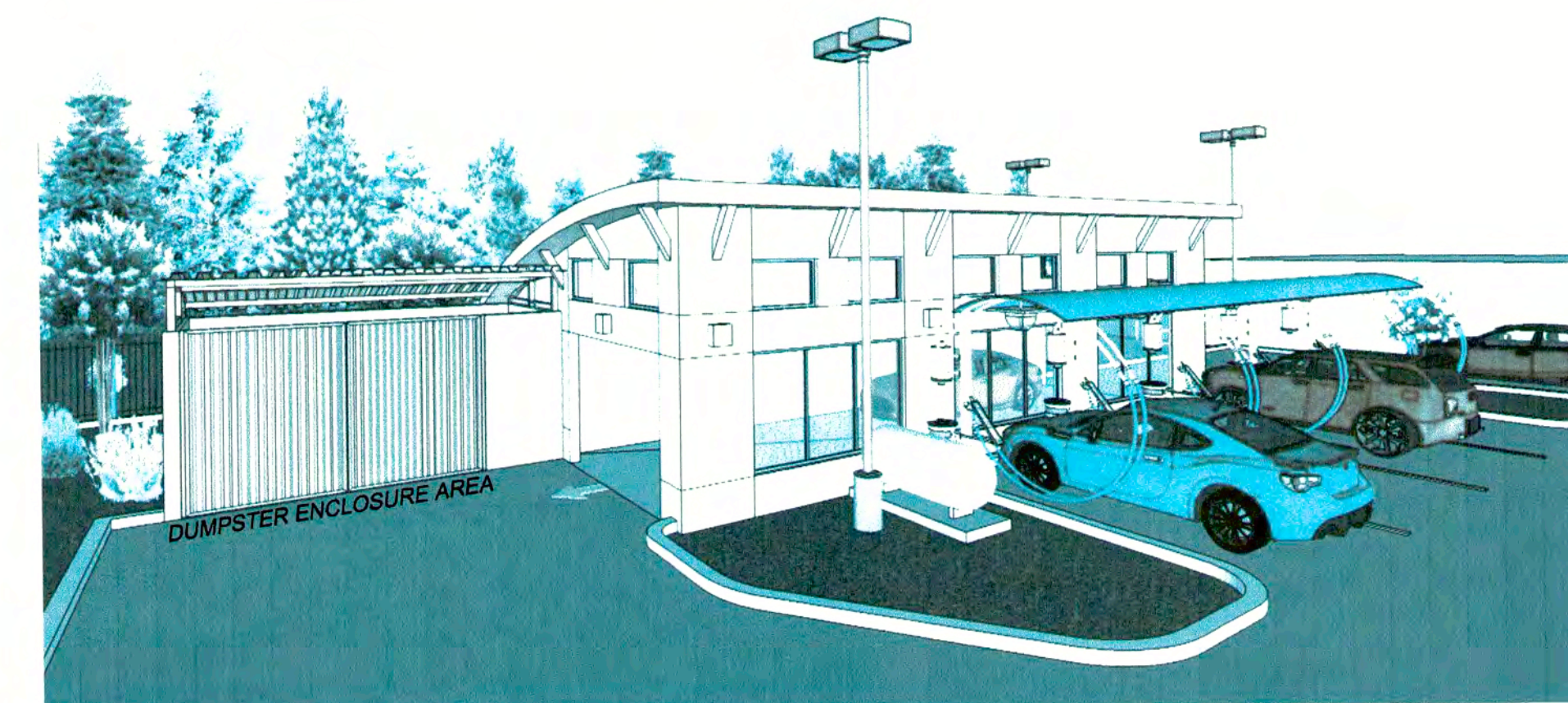
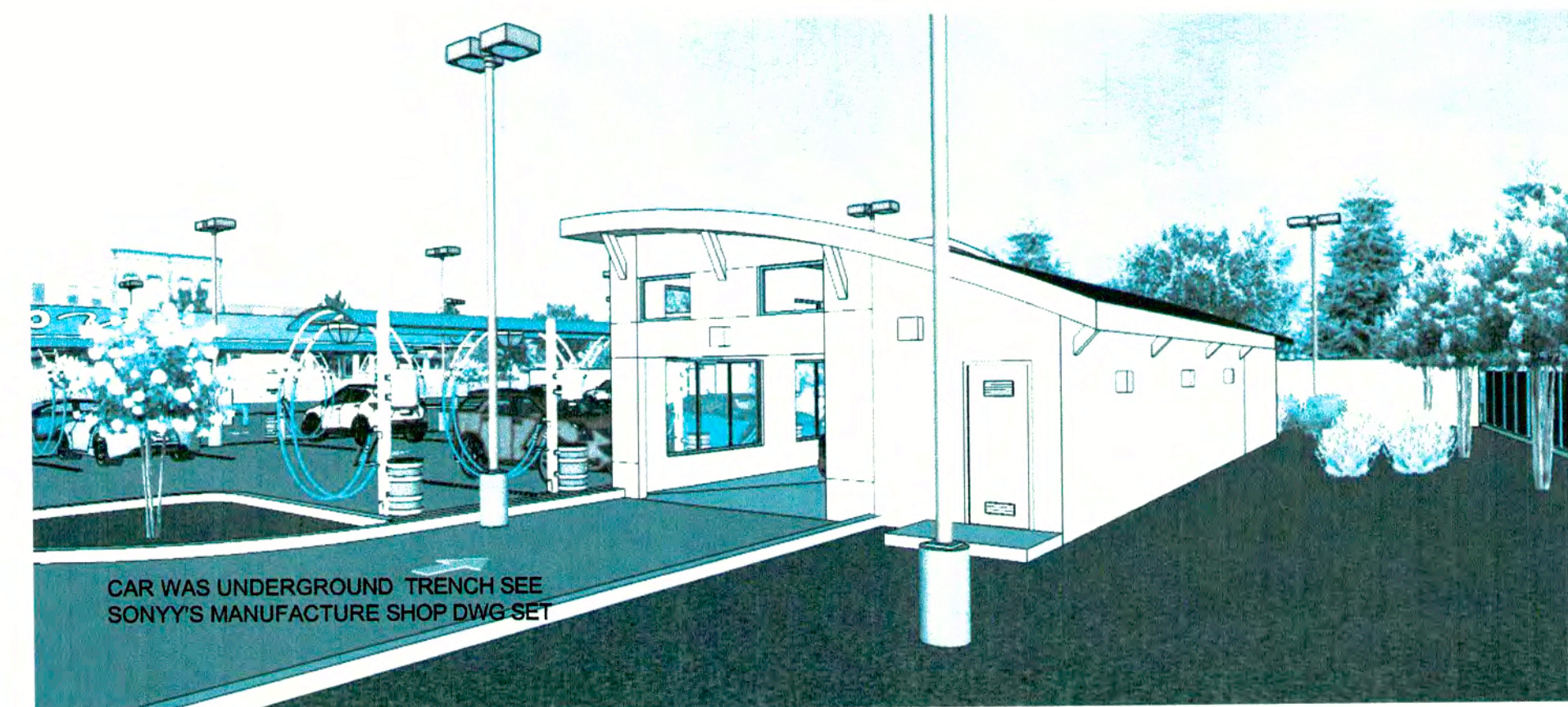
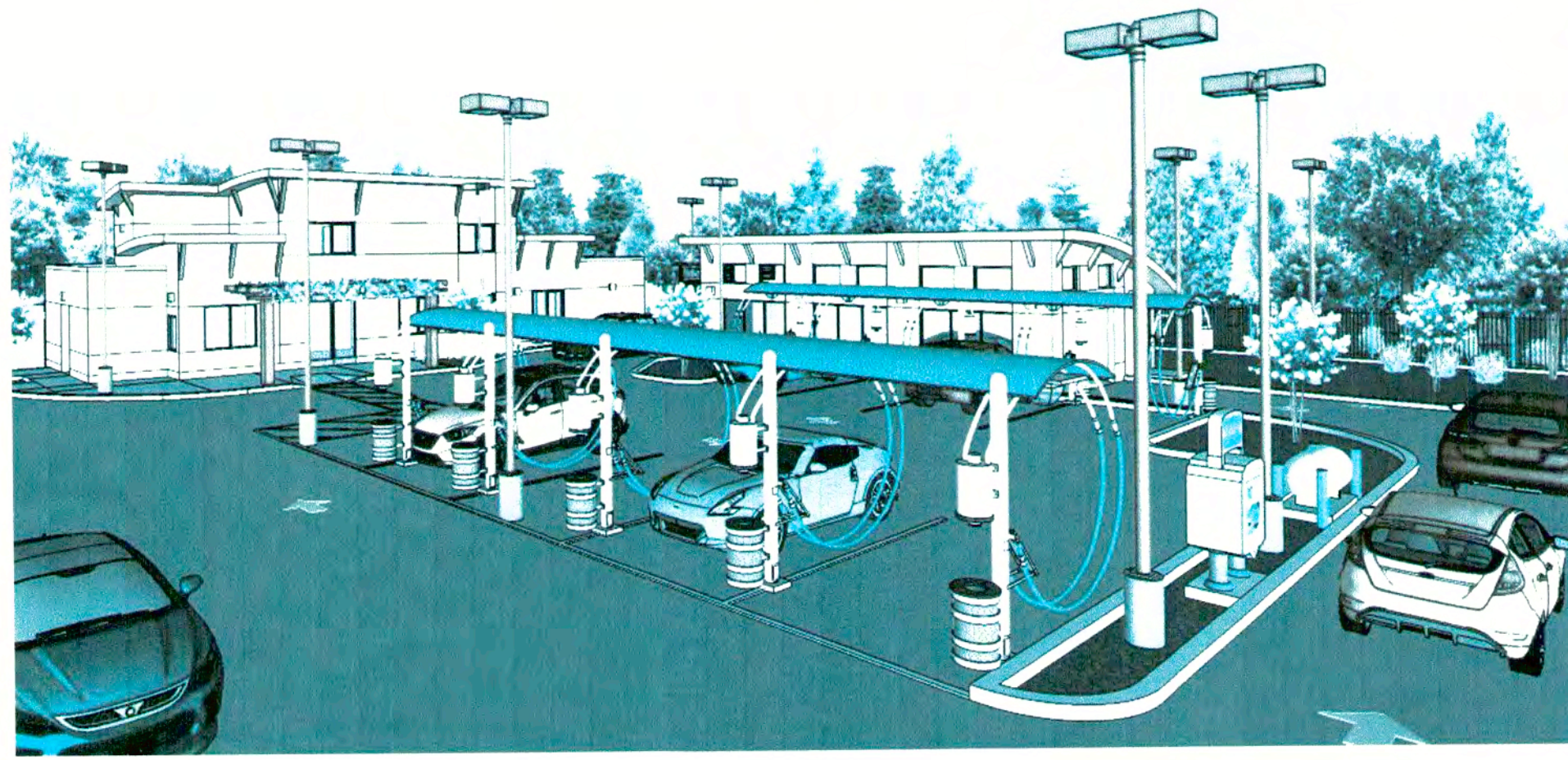


EMP FIXTURE LEGEND:

⊕	CEILING JUNCTION	⊠	2x2 AIR SUPPLY
⊕	WALL DUPLEX 15" A.F.F.	⊠	AIR RETURN
⊕	ABOVE TABLE	⊠	FIELD WATER BIBB OR UNDERCOUNTER WATER CONNECTION
⊕	UNDER COUNTER	FS	FLOOR SINK, (DRAIN LINES FROM SINK AND WALKING COOLER SHALL BE INDIRECT INTO FLOOR SINK W/ REQUIRED AIR GAP)
⊕	ABOVE SHELF	RD	ROOF DRAIN
⊕	HEAVY DUTY PLUG	FD	FLOOR DRAIN
⊕	(ALL PLUGS WITHIN FOOD PREPARATION AREA ARE GFI TYPE)	GFI	PLUG THRU COUNTER CONNECTED TO UNDER STRIP
⊕	WATER PROOF PLUG	—	T-8 FLUORESCENT TUBE OR SOFFIT MOUNT
⊕	GROUND FAULT PLUG	—	2x4 FLUORESCENT T-BAR OR SOFFIT MOUNT
⊕	SWITCH	—	2x2 FLUORESCENT T-BAR OR SOFFIT MOUNT
⊕	DIMER	—	SOFFIT SIDE SUPPLY
⊕	WALL MOUNTED SCENCE	—	SOFFIT SIDE RETURN
⊕	GAS VALVE	—	2x4 FLUORESCENT SUSPEND
⊕	TELEPHONE/ CABLE JACK		
⊕	RECESS LED		
⊕	CEILING MOUNTED DOWN LIGHT FOR FROZEN AREA		

RETAIL FIXTURES

⊕	SALAD/ SANDWICH PREPARATION TABLE	⊕	CONCORDIA COFFE SYSTEM
⊕	ENCLOSED BASE WORK TABLE	⊕	C713 SOFT SERVE FREEZER
⊕	UNDERCOUNTER ICE MACHINE	⊕	FROZEN BEVERAGES DISPENSERS
⊕	STEEL WORK TABLE W/ SINK	⊕	2x2 MOP SINK
⊕	ELECTRICAL FRYER	⊕	DISPLAY COOLER BOX
⊕	SELF-COOKING CENTER	⊕	FROZEN BOX
⊕	14 GA. S.5 WORK TABLE	⊕	GROCERY RACKS
⊕	S.S.3-COMPARTMENT SINK	⊕	6' L MAX PROPANE RACK
⊕	REACH-IN REFRIGERATOR/ FREEZER	⊕	WATER FILTRATION ANCHORED TO WALL
⊕	GLASS DISPLAY CASE (2)	⊕	WATER SOFTENER TANK
⊕	HOT DOG DISPLAY STEAMER	⊕	75 GA WATER HEATER (GAS TYPE)



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WEB: www.kcarchitects.net



ALL DIMENSIONS SHOWN WITH UNLESS OTHERWISE SPECIFIED ARE THE ORIGINAL AND UNREVISED DIMENSIONS OF THE PROJECT. THE ARCHITECT'S RESPONSIBILITY IS TO VERIFY THE ACCURACY OF ALL DIMENSIONS AND TO BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS MADE BY OTHER PROFESSIONALS OR CONTRACTORS IN CONNECTION WITH THIS PROJECT. ALL DIMENSIONS ON PLANS SHALL TAKE PRECEDENCE OVER ANY DIMENSIONS ON FIELD CONDITIONS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS, METHODS, TECHNIQUES, SAFETY AND PROTECTION OF THE WORK. THE ARCHITECT SHALL BE NOTIFIED OF ANY VARIATION OF DIMENSIONS AND CONDITIONS FROM THE AS SHOWN DOCUMENTS. SHOP DRAWINGS MUST BE REVIEWED BY THIS OFFICE BEFORE PROCEEDING WITH INSTALLATION AND FABRICATION.

GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD

Sheet Title
Date 03-10-16
Scale AS SHOWN
Drawn
Job
File
Sheet

A4

REVISIONS

NO.	DESCRIPTION

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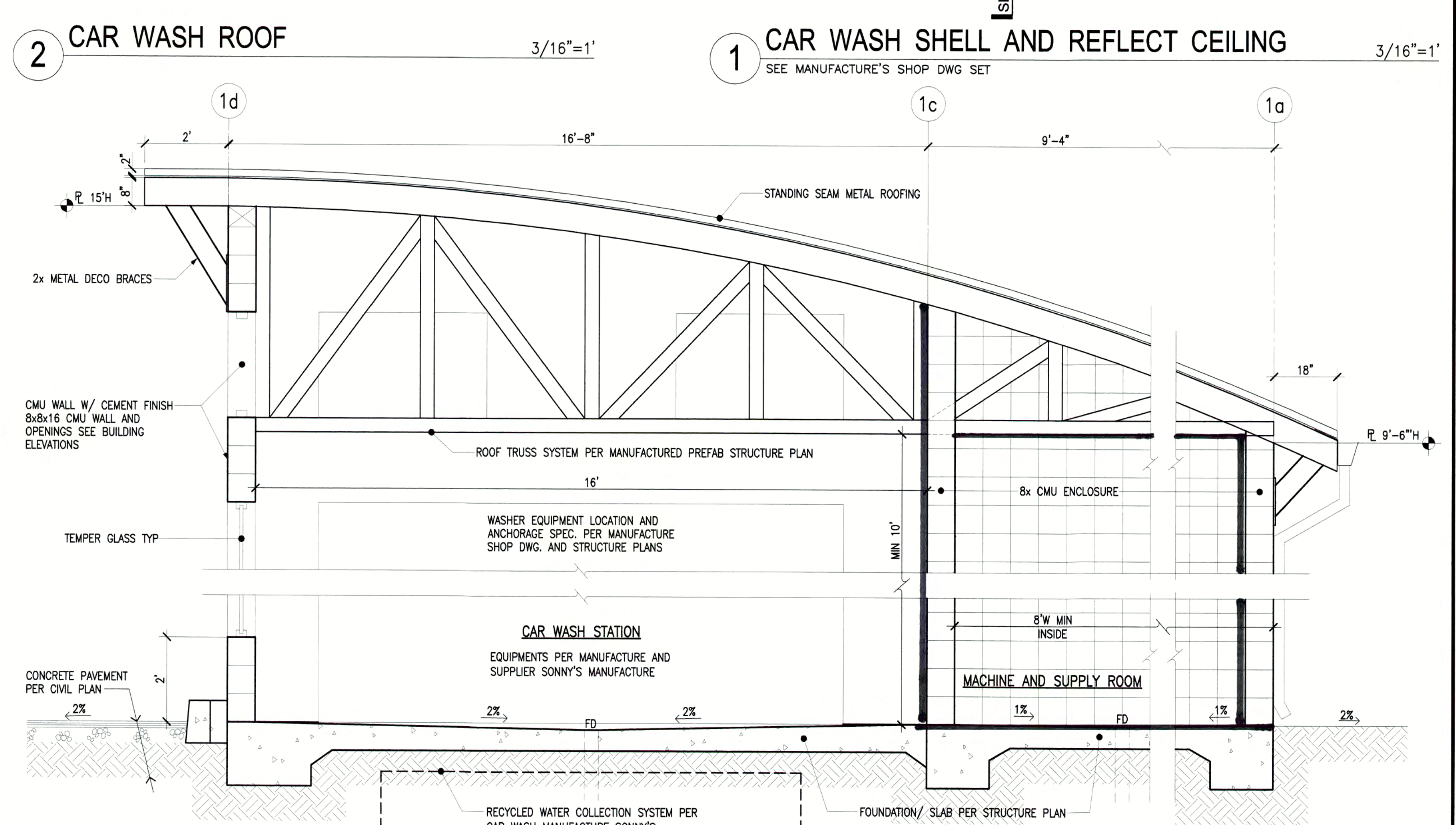
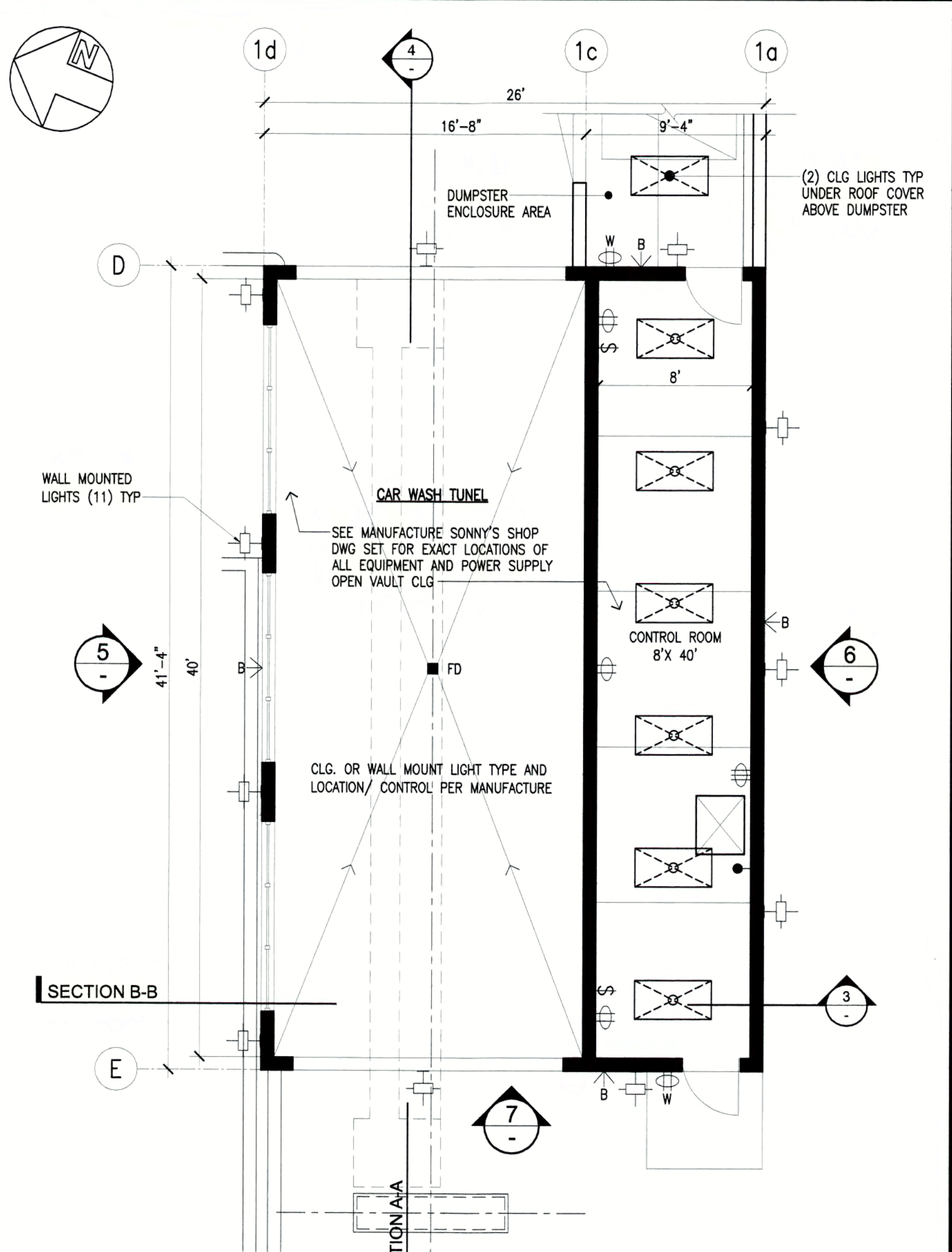
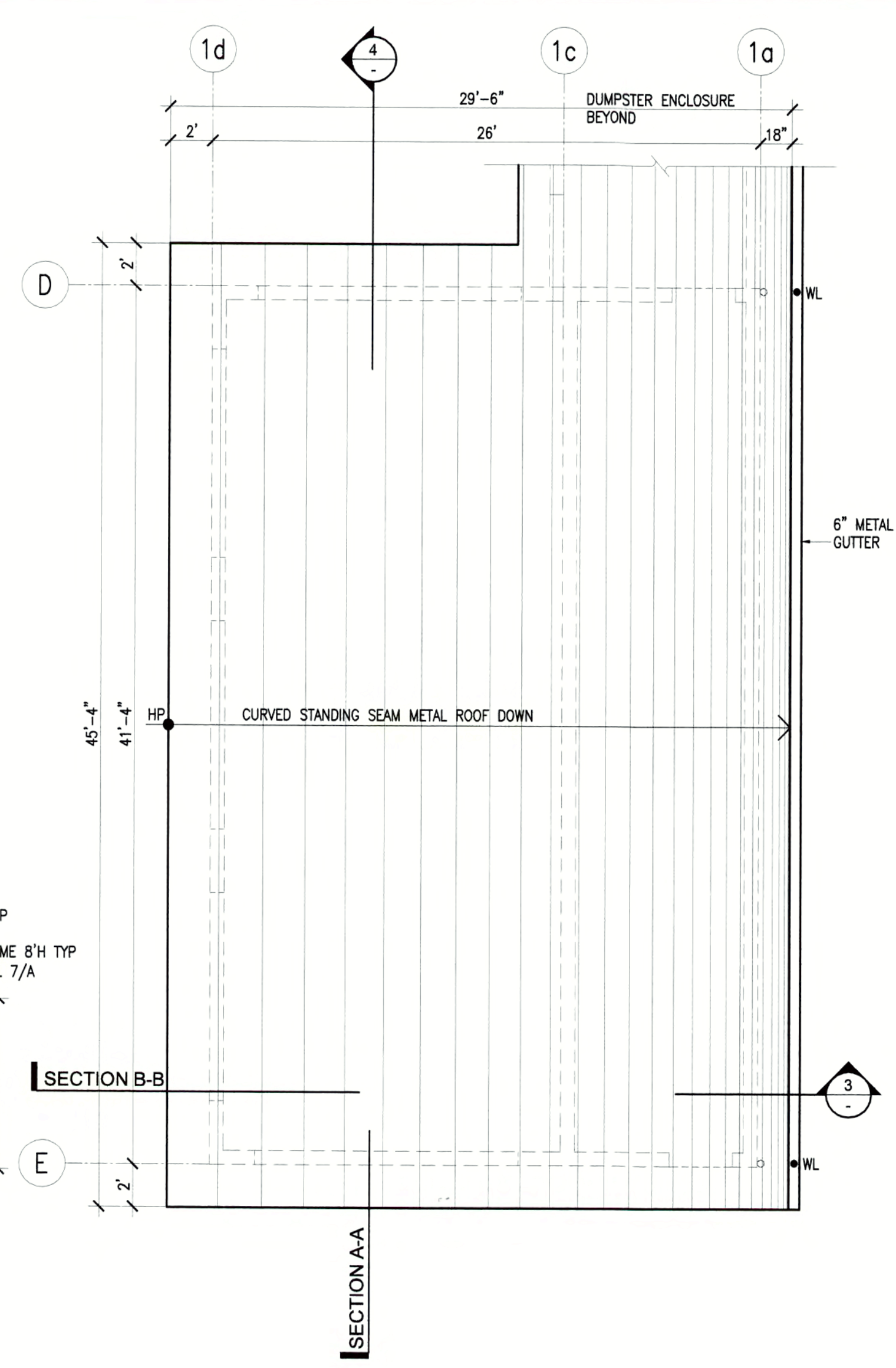
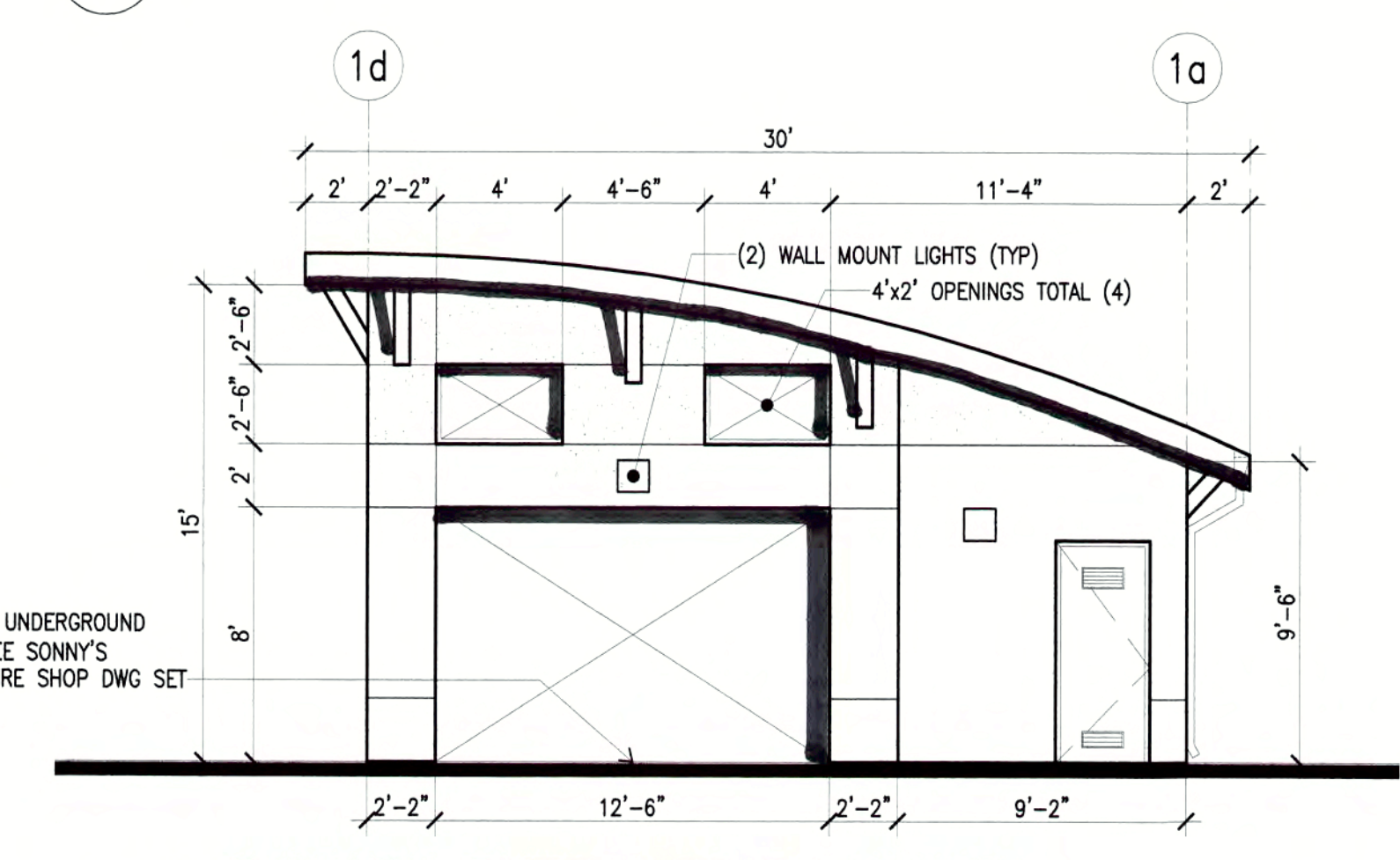
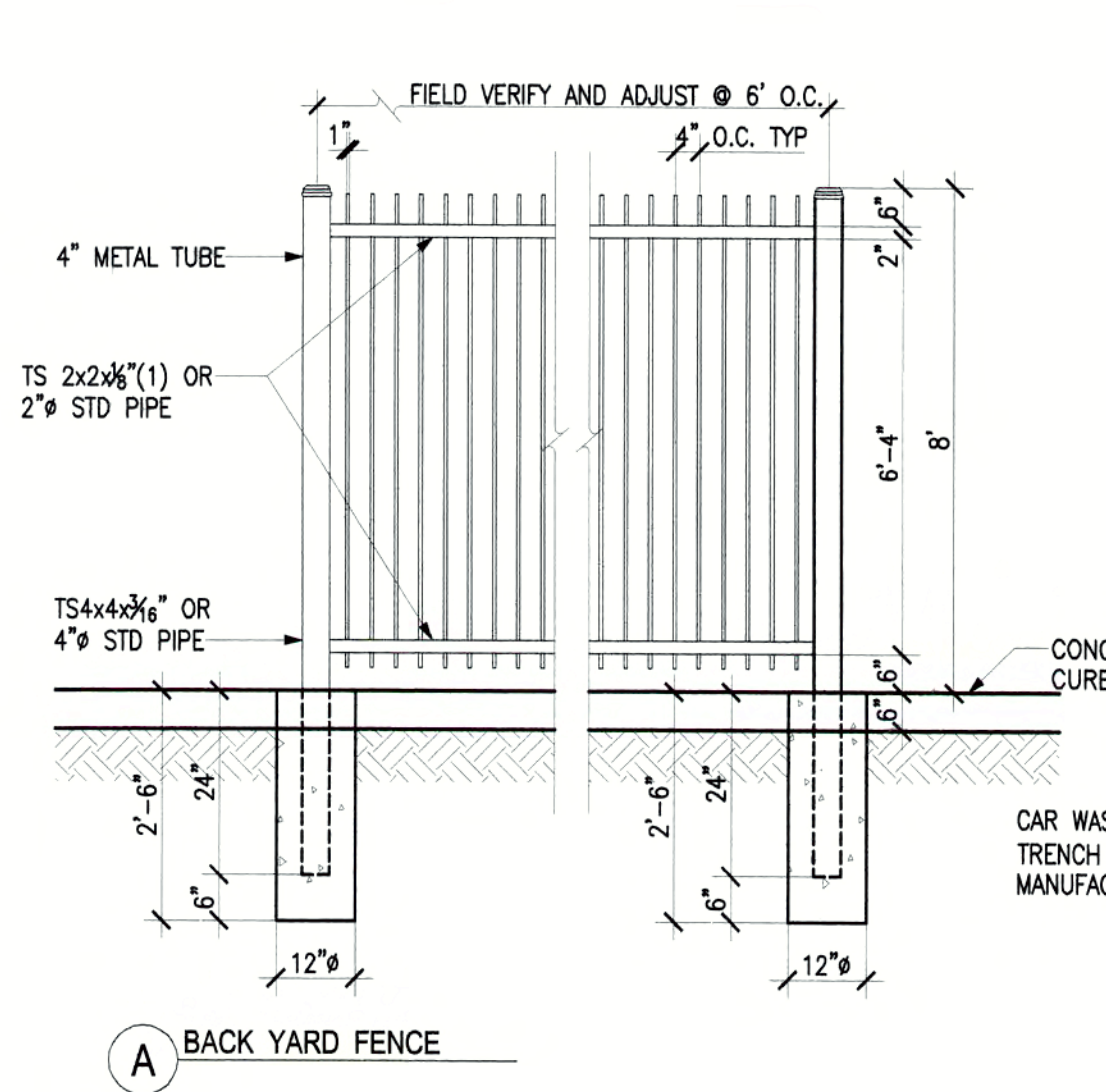
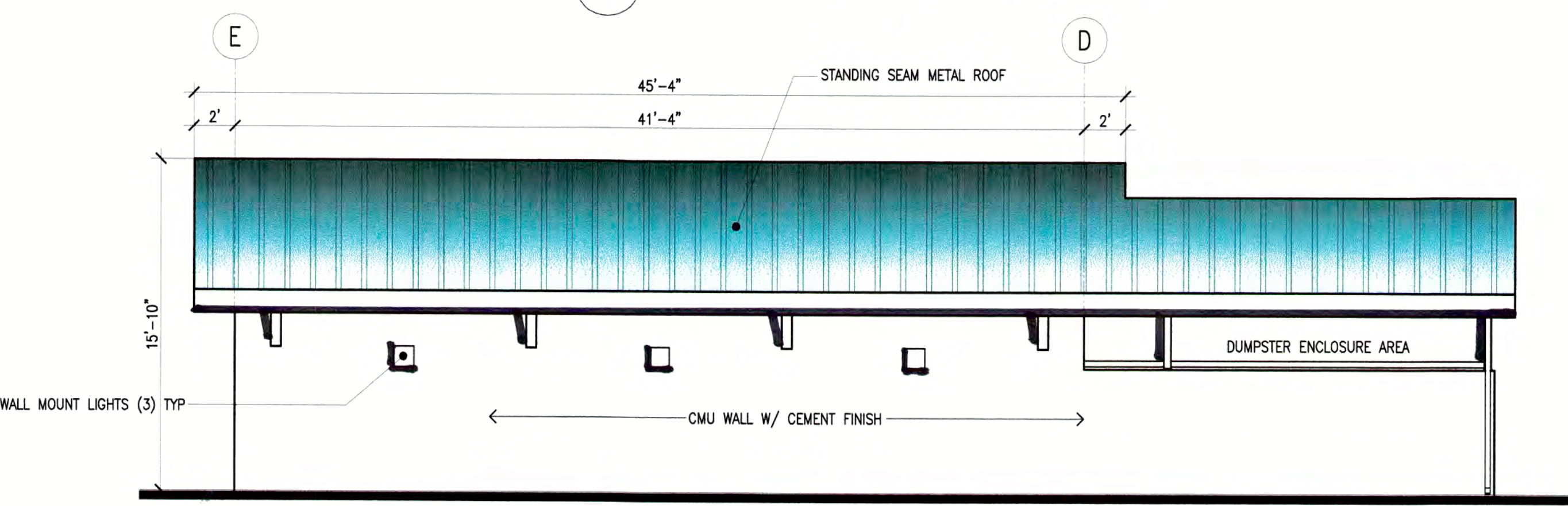
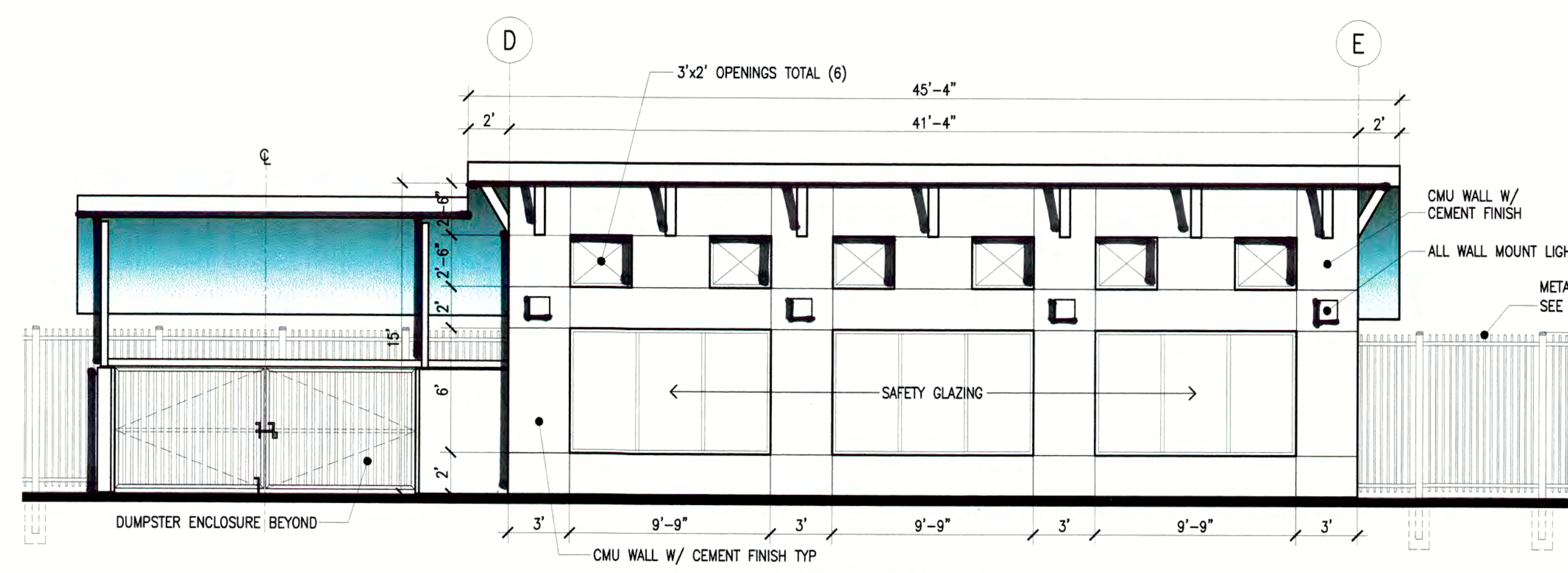
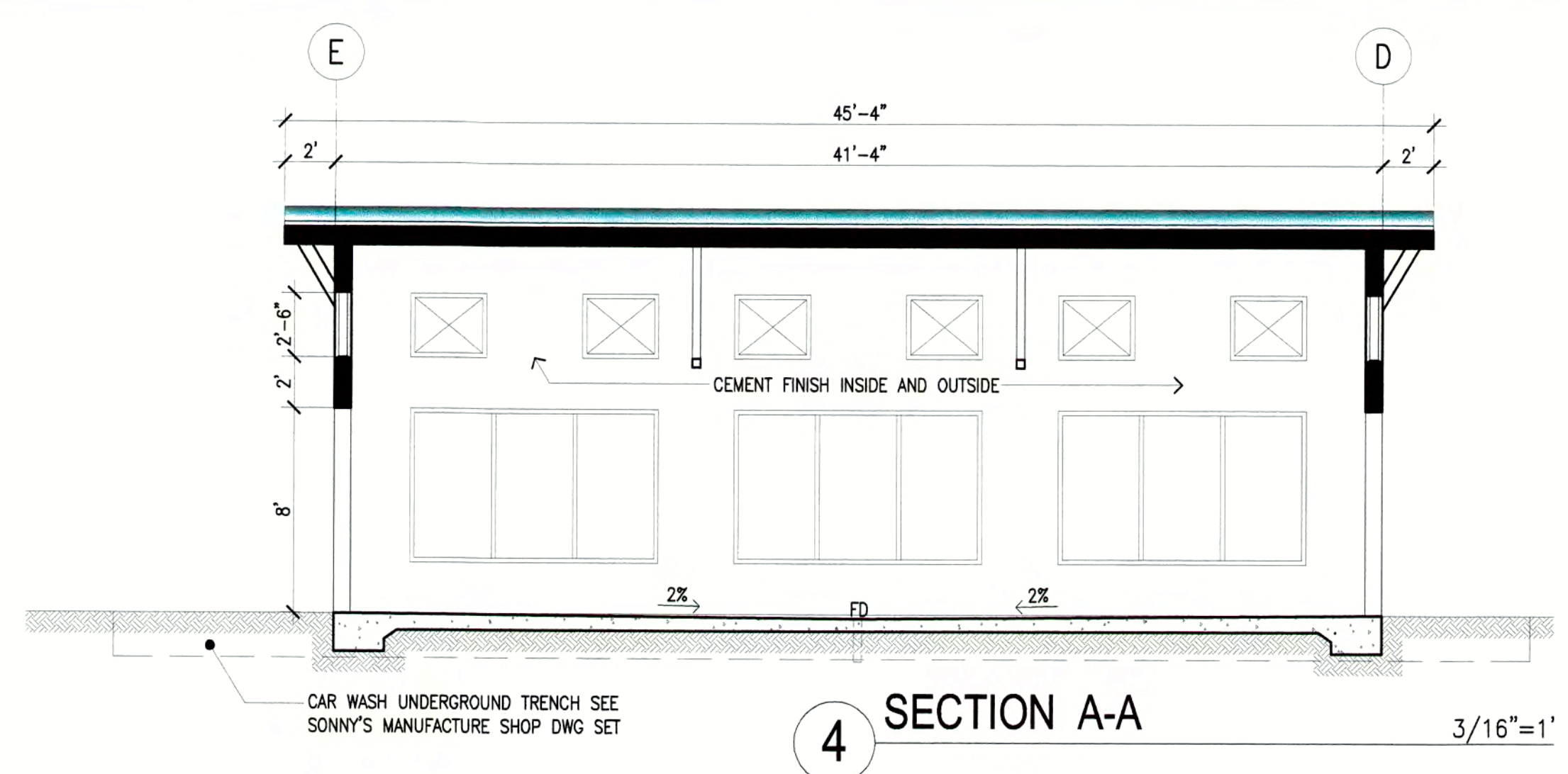


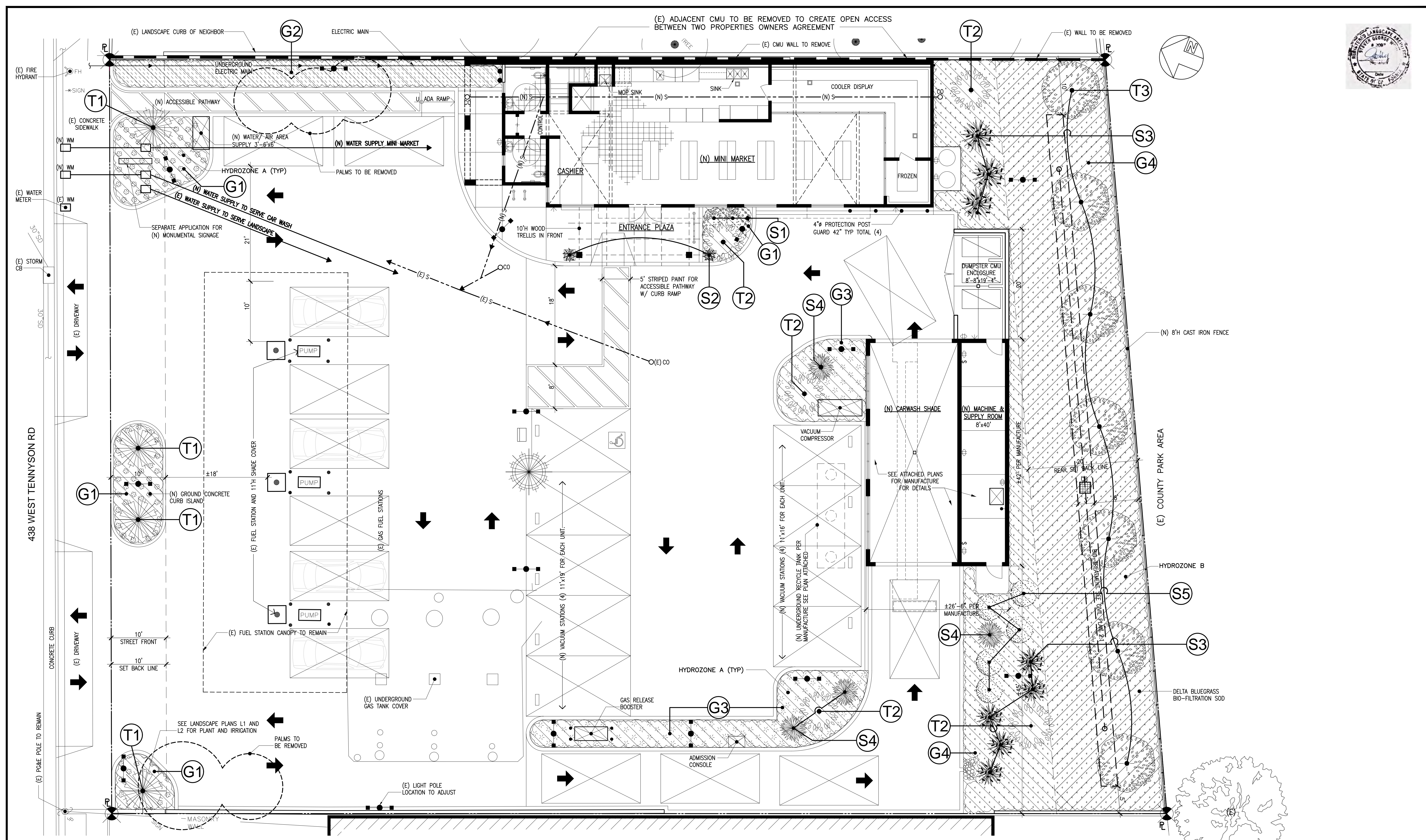
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**GAS STATION AND CAR WASH
 EXPANSION AND UPGRADE**
 438 WEST TENNYSON RD

Sheet Title
 Date 03-10-16
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 Job
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 Sheet

A5





REVISIONS

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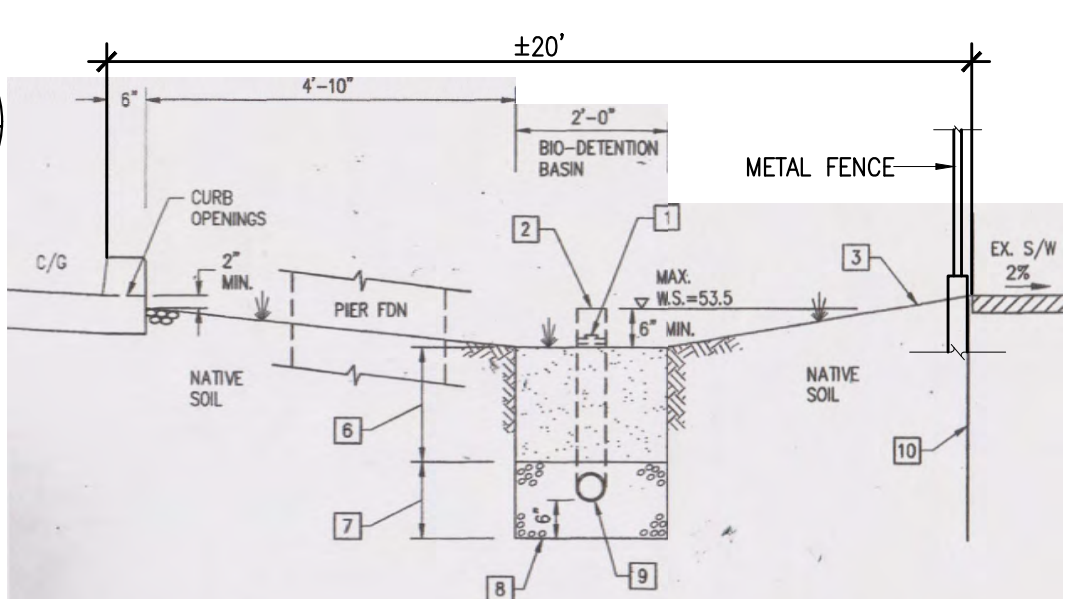
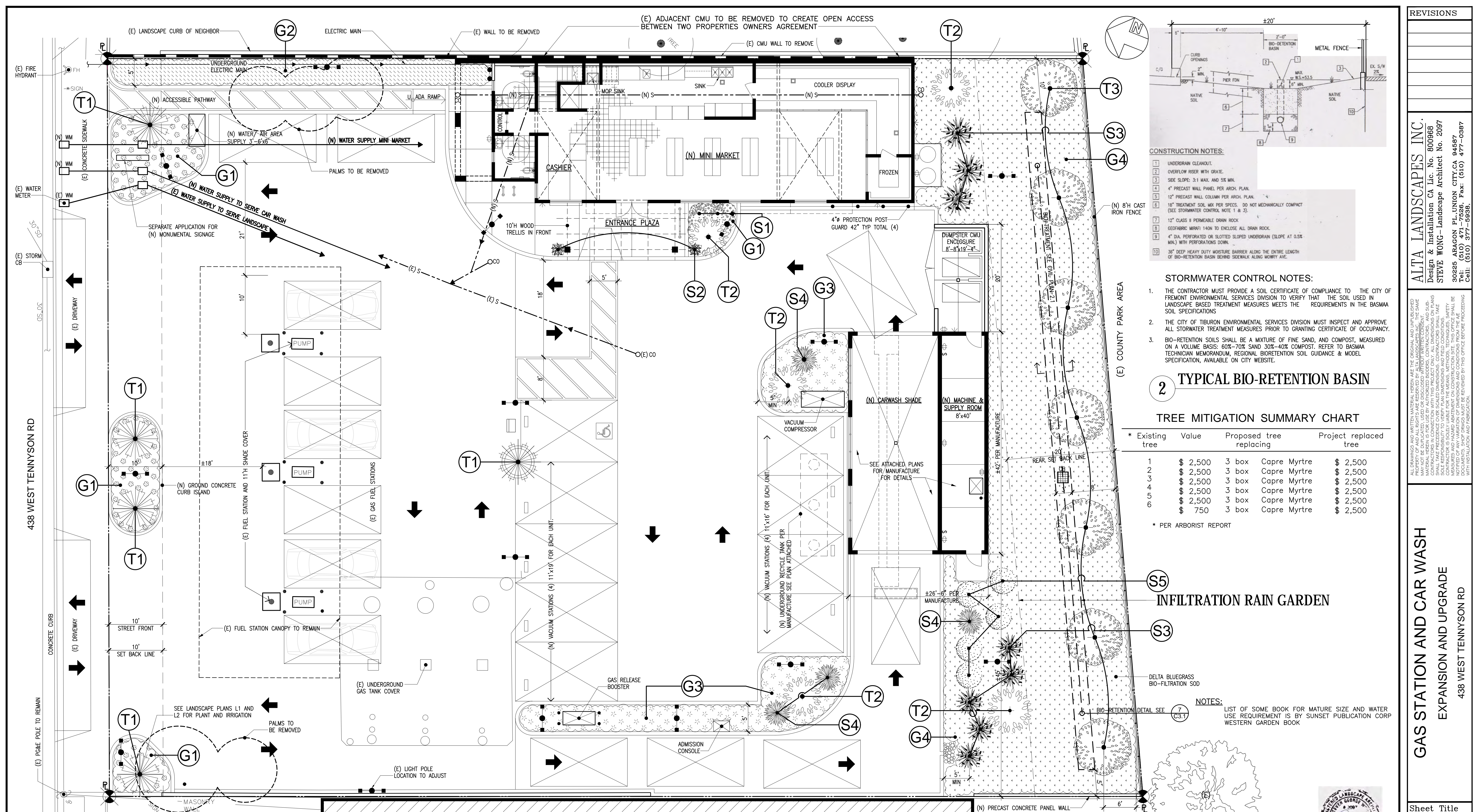
**GAS STATION AND CAR WASH
 EXPANSION AND UPGRADE**
 438 WEST TENNYSON RD

LEGEND

	HYDROZONE A DRIP IRRIGATION
	HYDROZONE B LOW PRECIPITATION SPRAYS

1 CONCEPTUAL IRRIGATION PLAN 1/8"=1'

Sheet Title	CONCEPTUAL IRRIGATION PLAN
Date	05-20-18
Scale	AS SHOWN
Drawn	
Job	
File	
Sheet	L2



- CONSTRUCTION NOTES:**
- UNDERDRAIN CLEANSOUT.
 - OVERFLOW RISER WITH GRATE.
 - SIDE SLOPE: 3:1 MAX. AND 5% MIN.
 - 4" PRECAST WALL PANEL PER ARCH. PLAN.
 - 12" PRECAST WALL COLUMN PER ARCH. PLAN.
 - 18" TREATMENT SOIL MAX PER SPECS. DO NOT MECHANICALLY COMPACT (SEE STORMWATER CONTROL NOTE 1 & 3).
 - 12" CLASS II PERMEABLE DRAIN ROCK.
 - GEOTEXTILE MESH 140# TO ENCLOSE ALL DRAIN ROCK.
 - 4" DIA. PERFORATED OR SLOTTED UNDERDRAIN (SLOPE AT 0.5% MAX.) WITH PERFORATIONS DOWN.
 - 36" DEEP HEAVY DUTY MOISTURE BARRIER ALONG THE ENTIRE LENGTH OF BIO-RETENTION BASIN BEHIND SIDEWALK ALONG MOWRY AVE.

- STORMWATER CONTROL NOTES:**
- THE CONTRACTOR MUST PROVIDE A SOIL CERTIFICATE OF COMPLIANCE TO THE CITY OF FREMONT ENVIRONMENTAL SERVICES DIVISION TO VERIFY THAT THE SOIL USED IN LANDSCAPE BASED TREATMENT MEASURES MEETS THE REQUIREMENTS IN THE BASMAA SOIL SPECIFICATIONS.
 - THE CITY OF TIBURON ENVIRONMENTAL SERVICES DIVISION MUST INSPECT AND APPROVE ALL STORMWATER TREATMENT MEASURES PRIOR TO GRANTING CERTIFICATE OF OCCUPANCY.
 - BIO-RETENTION SOILS SHALL BE A MIXTURE OF FINE SAND, AND COMPOST, MEASURED ON A VOLUME BASIS: 60%-70% SAND 30%-40% COMPOST. REFER TO BASMAA TECHNICIAN MEMORANDUM, REGIONAL BIORETENTION SOIL GUIDANCE & MODEL SPECIFICATION, AVAILABLE ON CITY WEBSITE.

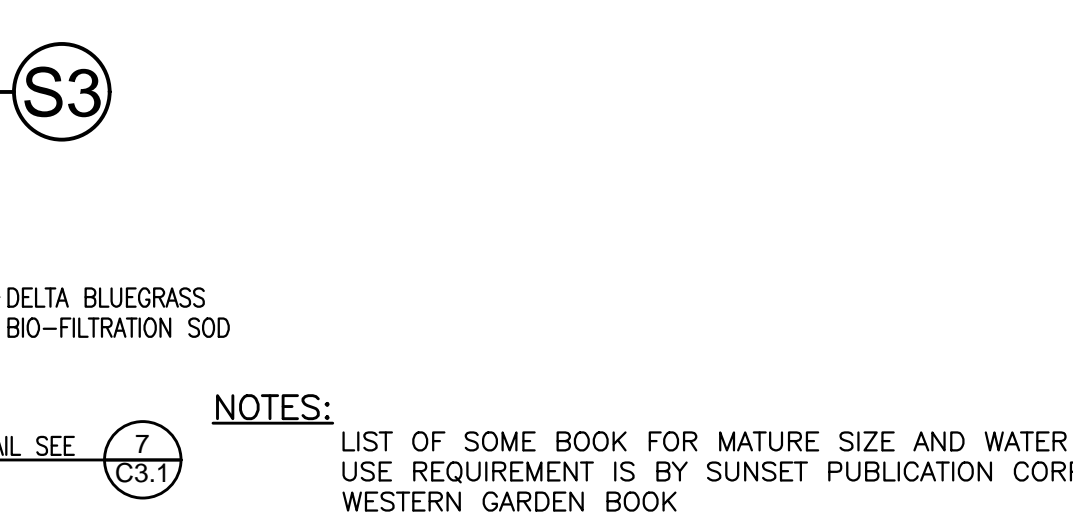
2 TYPICAL BIO-RETENTION BASIN

TREE MITIGATION SUMMARY CHART

* Existing tree	Value	Proposed tree replacing	Project replaced tree
1	\$ 2,500	3 box Capre Myrtle	\$ 2,500
2	\$ 2,500	3 box Capre Myrtle	\$ 2,500
3	\$ 2,500	3 box Capre Myrtle	\$ 2,500
4	\$ 2,500	3 box Capre Myrtle	\$ 2,500
5	\$ 2,500	3 box Capre Myrtle	\$ 2,500
6	\$ 750	3 box Capre Myrtle	\$ 2,500

* PER ARBORIST REPORT

INFILTRATION RAIN GARDEN



NOTES: LIST OF SOME BOOK FOR MATURE SIZE AND WATER USE REQUIREMENT IS BY SUNSET PUBLICATION CORP WESTERN GARDEN BOOK

PLANT LEGEND

Key	Size	Quan.	Botanical Name	Common Name	Mature Size	Plant space	Water Use Requirement
TREES:							
T1	36" Box	5	Trachycarpus fortunei	Windmill Palm	20-25"	20'	Low
T2	15 Gal	5	Lagerstroemia ind. "Catawba"	Crape Myrtle	15-20'	20'	Low
T3	15 Gal	7	Lagerstroemia ind. "Natchez"	Crape Myrtle	15-20'	20'	Low
SHRUBS:							
S1	5 Gal	4	Euryops pectinatus	Yellow Shrub Daisy	4'	4'	Low
S2	15 Gal	2	Bignonia cherere	Scarlet Trumpet Vine	20'	10'	Medium
S3	5 Gal	8	Echium fastuosum	Pride of Madeira	6"	6"	Low
S4	5 Gal	4	Phormium t. "Rainbow Warrior"	New Zealand Flax	6'	6'	Low

Key	Size	Quan.	Botanical Name	Common Name	Mature Size	Plant space	Water Use Requirement
SHRUBS:							
S5	5 Gal	5	Lavandula stoechess	Spanish Lavender	3'	4'	Low
GROUND COVERS:							
G1	1 Gal	4' o.c.	Lantana montevedensis	Lantana	2' ht. X 6'	4'	Low
G2	1 Gal	4' o.c.	Rosmarinus prostratus	Dwarf Rosemary	2' ht. X 6'	4'	Low
G3	1 Gal	4' o.c.	Cotoneaster "Lowfast"	Cotoneaster	1' ht. X 8'	4'	Low
G4	1 Gal	6' o.c.	Ceanothus h. "Yankee Point"	Wild Lilac	2-3' ht. X 8'	6'	Low
SOD: Delta Bluegrass "Bio-filtration Sod"							

1 LANDSCAPE PLAN 1/8"=1'

- NOTES:**
- MINIMUM DIMENSION FOR PLANTING AREA SHALL BE 5' MEASURED FROM BACK OF CURB/ PAVEMENT TO BACK OF CURB/ PAVEMENT
 - ALL PLANTS TO BE WATERED BY AUTOMATIC IRRIGATION SYSTEM.
 - ALL PLANTING AREAS TO RECEIVE 3" DEPTH BROWN SHREDDED MULCH
 - TOTAL LANDSCAPE AREA= 5,296 SF
LOT SIZE= 25,628 SF
PERCENTAGE OF LANDSCAPE= 5,296/ 25,628= 20%

REVISIONS

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GAS STATION AND CAR WASH EXPANSION AND UPGRADE
 438 WEST TENNYSON RD

Sheet Title
 CONCEPTUAL LANDSCAPE
 Date 05-20-18
 Scale AS SHOWN
 Drawn
 Job
 File
 Sheet **L1**

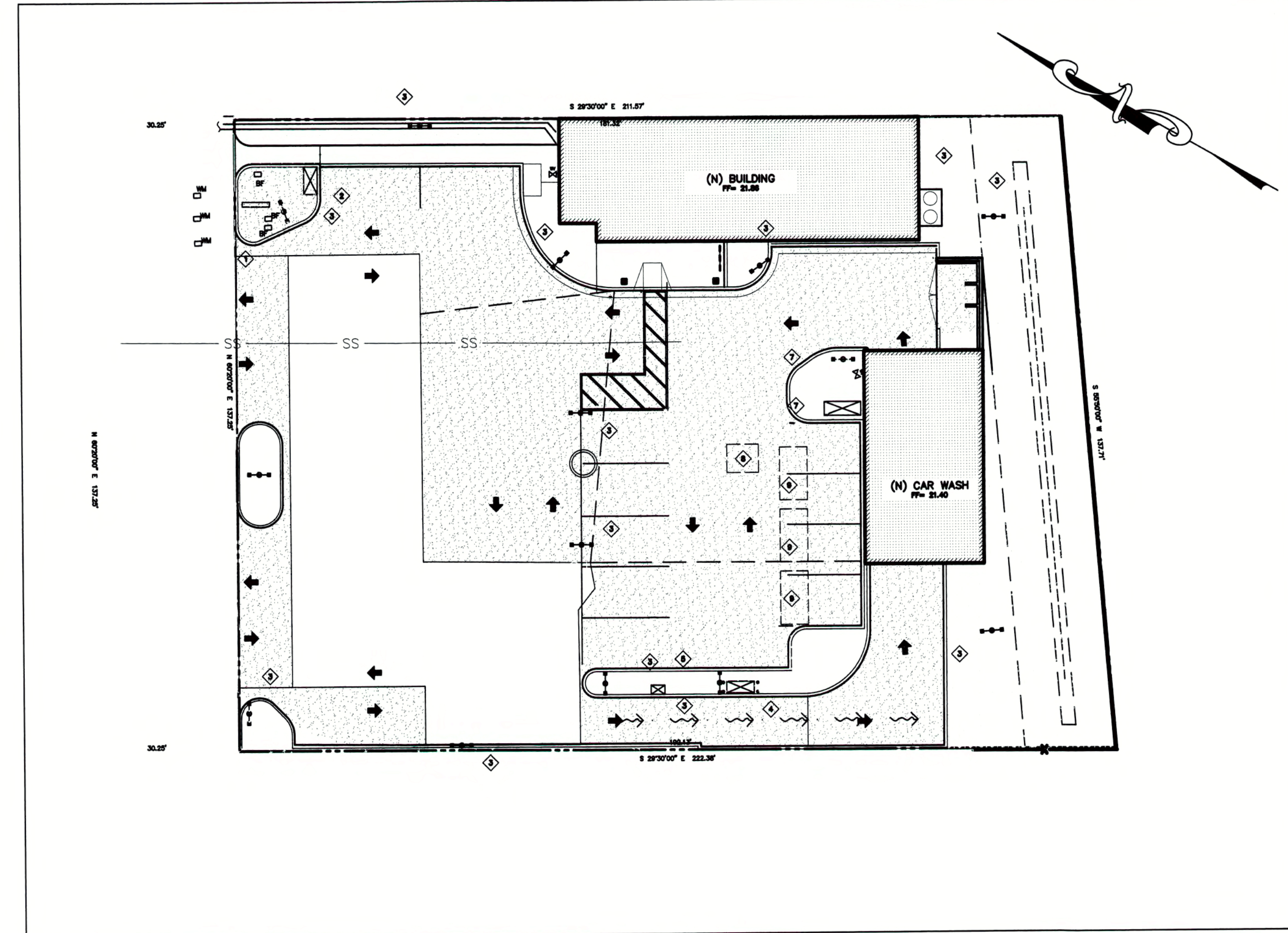
LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	BOUNDARY
---	---	PROPERTY LINE
---	---	RETAINING WALL
---	---	LANDSCAPE RETAINING WALL
---	---	SUBDRAIN LINE
---	---	TIGHTLINE
---	SD	STORM DRAIN LINE
---	SS	SANITARY SEWER LINE
---	W	WATER LINE
---	G	GAS LINE
---	P	PRESSURE LINE
---	JT	JOINT TRENCH
---	---	SET BACK LINE
---	---	CONCRETE VALLEY GUTTER
---	---	EARTHEN SWALE
CB	CB	CATCH BASIN
JB	JB	JUNCTION BOX
AD	AD	AREA DRAIN
SDMH	SDMH	STORM DRAIN MANHOLE
SSMH	SSMH	SANITARY SEWER MANHOLE
222.57 INV	222.57 INV	SPOT ELEVATION
←	←	FLOW DIRECTION
D	D	DEMOLISH/REMOVE
BENCHMARK	BENCHMARK	BENCHMARK
CONTOUR	CONTOUR	CONTOURS
TREE TO BE REMOVED	TREE TO BE REMOVED	TREE TO BE REMOVED
SWALE	SWALE	SWALE
DIRECTIONAL PAINT ARROW	DIRECTIONAL PAINT ARROW	DIRECTIONAL PAINT ARROW
CLEAN OUT TO GRADE	CLEAN OUT TO GRADE	CLEAN OUT TO GRADE

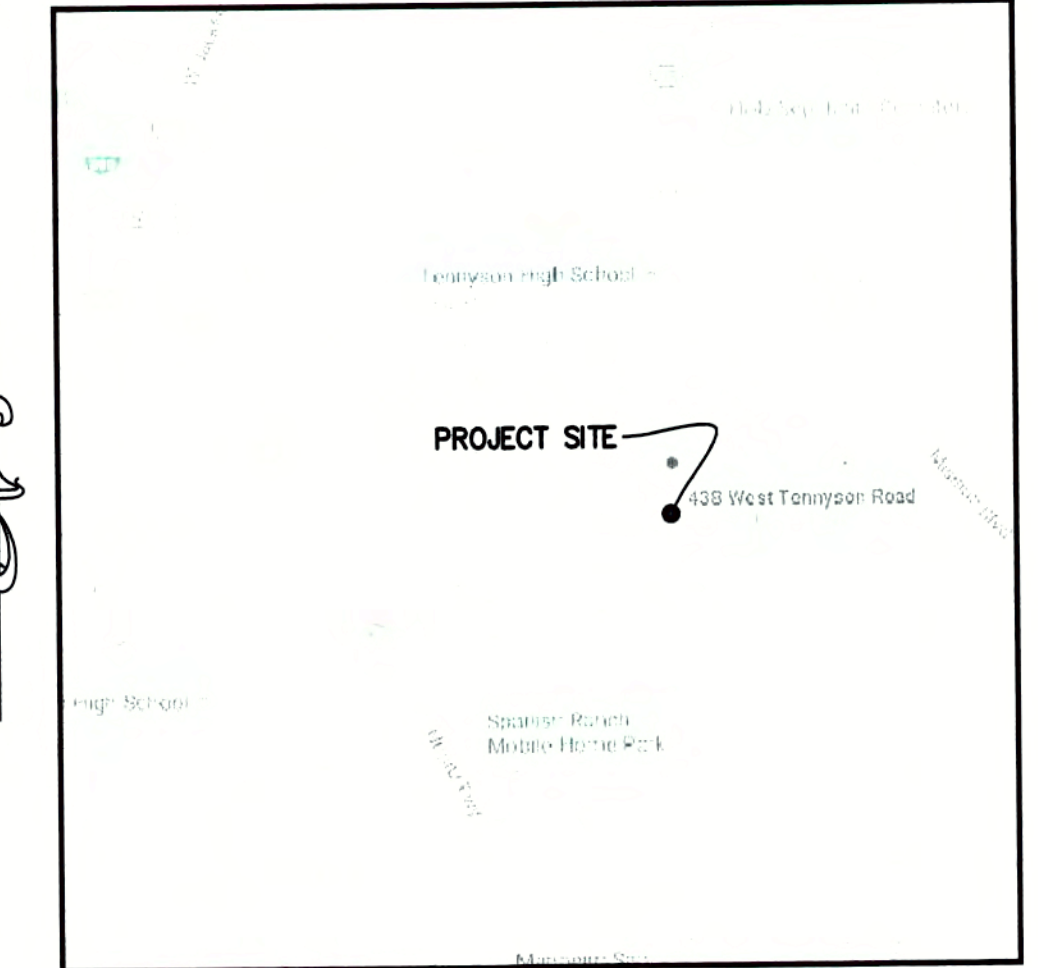
ABBREVIATIONS

AB	AGGREGATE BASE	LF	LINEAL FEET
AC	ASPHALT CONCRETE	MAX	MAXIMUM
ACC	ACCESSIBLE	MH	MANHOLE
AD	AREA DRAIN	MIN	MINIMUM
BC	BEGINNING OF CURVE	MON.	MONUMENT
B & D	BEARING & DISTANCE	(N)	NEW
BM	BENCHMARK	NO.	NUMBER
BW/FG	BOTTOM OF WALL/FINISH GRADE	NTS	NOT TO SCALE
CB	CATCH BASIN	O.C.	ON CENTER
C & G	CURB AND GUTTER	O/A	OVER
CL	CENTER LINE	P.A.	PLANTING AREA
CPP	CORRUGATED PLASTIC PIPE (SMOOTH INTERIOR)	PE	PEDESTRIAN
CO	CLEANOUT	PIV	POST INDICATOR VALVE
COTG	CLEANOUT TO GRADE	PSS	PUBLIC SERVICES EASEMENT
CONC	CONCRETE	R	PROPERTY LINE
CONST	CONSTRUCT or -TION	PP	POWER POLE
CONC COR	CONCRETE CORNER	PUE	PUBLIC UTILITY EASEMENT
CY	CUBIC YARD	PVC	POLYVINYL CHLORIDE
D	DIAMETER	R	RADIUS
DI	DROP INLET	RCP	REINFORCED CONCRETE PIPE
DIP	DUCTILE IRON PIPE	RIW	RIM ELEVATION
EA	EACH	RW	RAINWATER
EC	END OF CURVE	R/W	RIGHT OF WAY
EG	EXISTING GRADE ELEVATIONS	S	SLOPE
EL	ELEVATIONS	S.D.	SEE ARCHITECTURAL DRAWINGS
EP	EDGE OF PAVEMENT	SAN	SANITARY
EQ	EQUIPMENT	SD	STORM DRAIN
EW	EACH WAY	SDMH	STORM DRAIN MANHOLE
(E)	EXISTING	SHT	SHEET
FC	FACE OF CURB	S.L.D.	SEE LANDSCAPE DRAWINGS
FF	FINISHED FLOOR	SPEC	SPECIFICATION
FG	FINISHED GRADE	SS	SANITARY SEWER
FH	FIRE HYDRANT	SSCO	SANITARY SEWER CLEANOUT
FL	FLOW LINE	SSMH	SANITARY SEWER MANHOLE
FS	FINISHED SURFACE	ST	STREET
G	GAS	STA	STATION
GA	GAGE OR GAUGE	STD	STANDARD
GB	GRADE BREAK	STRUCT	STRUCTURAL
HDPE	HIGH DENSITY CORRUGATED POLYETHYLENE PIPE	T	TELEPHONE
HORIZ	HORIZONTAL	TC	TOP OF CURB
HI PT	HIGH POINT	TEMP	TEMPORARY
H&T	HUB & TACK	TP	TOP OF PAVEMENT
ID	INSIDE DIAMETER	TS	TOP OF SLOPE
INV	INVERT ELEVATION	TW/FG	TOP OF WALL/FINISH GRADE
JB	JUNCTION BOX	TYP	TYPICAL
JT	JOINT TRENCH	VC	VERTICAL CURVE
JP	JOINT UTILITY POLE	VCP	VITRIFIED CLAY PIPE
L	LENGTH	VERT	VERTICAL
LNDG	LANDING	W	WITH
		W, WL	WATER LINE
		WM	WATER METER
		WWF	WELDED WIRE FABRIC

GAS STATION AND CAR WASH EXPANSION AND UPGRADE 438 WEST TENNYSON RD. HAYWARD, CALIFORNIA



KEY MAP
1" = 20'



VICINITY MAP
NTS

REFERENCES

- THIS GRADING AND DRAINAGE PLAN IS SUPPLEMENTAL TO:
1. TOPOGRAPHIC SURVEY BY DAINS LAND SURVEYING, ENTITLED: "TOPOGRAPHIC SURVEY PLAN" 438 WEST TENNYSON ROAD HAYWARD CALIFORNIA, USA DATED: 3-10-15 JOB# 2015027564
 2. SITE PLAN BY KC ASSOCIATES ENTITLED: "GAS STATION AND CAR WASH" EXPANSION AND UPGRADE 438 WEST TENNYSON ROAD HAYWARD CALIFORNIA, USA
- THE CONTRACTOR SHALL REFER TO THE ABOVE NOTED SURVEY AND PLAN, AND SHALL VERIFY BOTH EXISTING AND PROPOSED ITEMS ACCORDING TO THEM.

*** BUILDING PAD NOTE:**
ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR CRAWL SPACE DEPTH TO ESTABLISH PAD LEVEL.



SHEET INDEX

C0.1	TITLE SHEET
C0.2	GRADING SPECIFICATIONS
C1.0	DEMOLITION PLAN
C2.1	GRADING & DRAINAGE PLAN
C3.1	DETAILS
C3.2	DETAILS
C4.1	EROSION CONTROL
C4.2	EROSION CONTROL DETAILS
C4.3	BEST MANAGEMENT PRACTICES (SWPPP)



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10700 Highway One, Suite 100, Hayward, CA
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**GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD.
HAYWARD, CALIFORNIA**
 APN: 465-0001-002
 ALAMEDA COUNTY

TITLE SHEET

REVISIONS	BY
JOB NO:	216026
DATE:	3-14-18
SCALE:	AS NOTED
DESIGN BY:	WCC
DRAWN BY:	DR
SHEET NO:	CO.1

GENERAL SITE NOTES:

- 1. CONTRACTOR SHALL VISIT THE SITE PRIOR TO BIDDING ON THIS WORK AND CONSIDER THE EXISTING CONDITIONS AND SITE CONSTRAINTS IN THE BID. CONTRACTOR SHALL BE IN THE POSSESSION OF AND FAMILIAR WITH ALL APPLICABLE GOVERNING AGENCIES STANDARD DETAILS AND SPECIFICATIONS PRIOR TO SUBMITTING OF A BID.
2. ALL WORK IN THE PUBLIC RIGHT-OF-WAY SHALL CONFORM TO ALL APPLICABLE GOVERNING AGENCIES STANDARD DETAILS & SPECIFICATIONS.
3. PRIOR TO BEGINNING WORK, AND AFTER INITIAL HORIZONTAL CONTROL STAKING, CONTRACTOR SHALL FIELD CHECK ALL ELEVATIONS MARKED WITH (E) AND REPORT ANY DISCREPANCIES GREATER THAN 0.05' TO OWNER'S PROJECT MANAGER AND CIVIL ENGINEER.
4. DAMAGE TO ANY EXISTING SITE IMPROVEMENTS, UTILITIES AND/OR SERVICES TO REMAIN SHALL BE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL REPAIR AND/OR REPLACE IN KIND.
5. CONTRACTOR SHALL REPLACE ALL STRUCTURES AND GRATE LIDS FOR VAULTS, CATCH BASINS, ETC., WITH VEHICULAR-RATED STRUCTURES IN ALL TRAFFIC ACCESSIBLE AREAS WITHIN NEW CONSTRUCTION AREA UNLESS OTHERWISE NOTED.
6. THE CONTRACTOR SHALL ADJUST TO FINAL GRADE ALL EXISTING AND/OR NEW MANHOLES, CURB INLETS, CATCH BASIN, VALVES, MONUMENT COVERS, AND OTHER CASTINGS WITHIN THE CONSTRUCTION AREA TO FINAL GRADE IN PAVEMENT AND LANDSCAPE AREAS UNLESS OTHERWISE NOTED.
7. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT TO BE LIMITED TO NORMAL WORKING HOURS AND THAT THE CONTRACTOR SHALL DEFEND INDEMNIFY AND HOLD THE OWNER, THE CONSULTING ENGINEER AND THE CITY HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT. EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE CONSULTING ENGINEER.
8. EXISTING PEDESTRIAN WALKWAYS, BIKE PATHS AND ACCESSIBLE PATHWAYS SHALL BE MAINTAINED, WHERE FEASIBLE, DURING CONSTRUCTION.
9. IF A CONFLICT ARISES BETWEEN THE SPECIFICATIONS AND THE PLANS NOTES, THE MORE STRINGENT REQUIREMENT SHALL GOVERN.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY REQUIRED PERMITS AND COSTS ASSOCIATED WITH SAID PERMITS

TREE/PLANT PROTECTION NOTES:

- 1. PRIOR TO BEGINNING CONSTRUCTION ON SITE, CONTRACTOR SHALL IDENTIFY, CONFIRM WITH OWNER AND PROTECT EXISTING TREES AND PLANTS DESIGNATED AS TO REMAIN.
2. PROVIDE 5 FOOT TALL TREE PROTECTION FENCE WITH DISTINCTIVE MARKING VISIBLE TO CONSTRUCTION EQUIPMENT, ENCLOSING DRIP LINES OF TREES DESIGNATED TO REMAIN.
3. WORK REQUIRED WITHIN FENCE LINE SHALL BE HELD TO A MINIMUM, AVOID UNNECESSARY MOVEMENT OF HEAVY EQUIPMENT WITHIN FENCED AREA AND DO NOT PARK ANY VEHICLES UNDER DRIP LINE OR TREES. DO NOT STORE EQUIPMENT OR MATERIALS WITHIN FENCE LINE.
4. PRIOR TO REMOVING ROOTS AND BRANCHES LARGER THAN 2" IN DIAMETER OF TREES OR PLANTS THAT ARE TO REMAIN, CONSULT WITH THE OWNER'S PROJECT MANAGER.
5. ANY GRADE CHANGES GREATER THAN 6" WITHIN THE DRIPLINE OF EXISTING TREES SHALL NOT BE MADE WITHOUT FIRST CONSULTING THE ARCHITECT / CIVIL ENGINEER.
6. PROTECT EXISTING TREES TO REMAIN FROM SPILLED CHEMICALS, FUEL OIL, MOTOR OIL, GASOLINE AND ALL OTHER CHEMICALLY INJURIOUS MATERIALS; AS WELL AS FROM PUDDLING OR CONTINUOUSLY RUNNING WATER. SHOULD A SPILL OCCUR, STOP WORK IN THAT AREA AND CONTACT THE INSPECTOR IMMEDIATELY. CONTRACTOR SHALL BE RESPONSIBLE TO MITIGATE DAMAGE FROM SPILLED MATERIAL AS WELL AS MATERIAL CLEAN UP.
7. PROVIDE TEMPORARY IRRIGATION TO ALL TREES AND PLANTS THAT ARE IN OR ADJACENT TO CONSTRUCTION AREAS WHERE EXISTING IRRIGATION SYSTEMS MAY BE AFFECTED BY THE CONSTRUCTION. ALSO PROVIDE TEMPORARY IRRIGATION TO RELOCATE TREES.
8. CONTRACTOR SHALL BE RESPONSIBLE FOR ONGOING MAINTENANCE OF ALL TREES AND PLANTS DESIGNATED TO REMAIN AND FOR MAINTENANCE OF RELOCATED TREES STOCKPILED DURING CONSTRUCTION. CONTRACTOR WILL BE REQUIRED TO REPLACE TREES OR PLANTS THAT DIE DUE TO LACK OF MAINTENANCE.
9. TREE PROTECTION ZONES NEED TO BE SET UP WITH FENCING AROUND TREES TO A MINIMUM DISTANCE OF 10 FEET FROM THE BUTTRESS FLAIR. NO EQUIPMENT, MATERIALS STORAGE, OR DIGGING IS ALLOWED WITHIN THE TREE PROTECTION ZONE WITHOUT WRITTEN AUTHORIZATION FROM THE PROJECT ARBOHIST, ARBOHIST SUPERVISOR OR AUTHORIZED DESIGNATE. ANY AUTHORIZED DIGGING WITHIN THE TREE PROTECTION ZONE MUST BE DONE BY HAND, I.E. PICK AND SHOVEL. CARE MUST BE TAKEN TO AVOID SEVERING ANY STRUCTURAL ROOTS. ANY ROOTS GREATER THAN 2" IN DIAMETER INCIDENTALLY SEVERED, WHETHER INSIDE OR OUTSIDE OF THE TREE PROTECTION ZONE, WILL NEED TO BE BROUGHT TO THE ATTENTION OF AND INSPECTED BY THE PROJECT ARBOHIST, ARBOHIST SUPERVISOR OR AUTHORIZED DESIGNATE, WHO WILL EVALUATE THE TREE IN QUESTION FOR IMPACTS TO BOTH LONG TERM HEALTH AND STABILITY. ANY ROOT SEVERANCE CONCLUDED TO COMPROMISE TREE STABILITY/SAFETY MAY RESULT IN TREE REMOVAL. ANY COSTS RESULTING FROM TREE REMOVALS WILL BE CHARGED TO THE PROJECT IN QUESTION. ANY COSTS FROM TREE REMOVALS RESULTING FROM VIOLATIONS OF THE COUNTY CODES WILL BE ABSORBED BY THE CONTRACTOR UP TO AND INCLUDING ANY FINES LEVIED BY THE COUNTY.

SITE MAINTENANCE:

- 1. REMOVE ALL DIRT, GRAVEL, RUBBISH, REFUSE, AND GREEN WASTE FROM STREET PAVEMENT AND STORM DRAINS ADJOINING THE SITE. LIMIT CONSTRUCTION ACCESS ROUTES ONTO THE SITE AND PLACE GRAVEL PADS AT THESE LOCATIONS. DO NOT DRIVE VEHICLES AND EQUIPMENT OFF THE PAVED OR GRAVELED AREAS DURING WET WEATHER.
2. SWEEP OR VACUUM THE STREET PAVEMENT AND SIDEWALKS ADJOINING THE PROJECT SITE AND THE ON-SITE PAVED AREAS ON A DAILY BASIS. SCRAPE CAKED-ON MUD AND DIRT FROM THESE AREAS BEFORE SWEEPING. CORNERS AND HARD TO REACH AREAS SHALL BE SWEEPED MANUALLY.
3. CONTRACTOR SHALL: GATHER ALL CONSTRUCTION DEBRIS ON A REGULAR BASIS AND PLACE IT IN A DUMPSTER OR OTHER CONTAINER WHICH IS EMPTIED OR REMOVED ON A REGULAR BASIS. WHEN APPROPRIATE, USE TARPS ON THE GROUND TO COLLECT FALLEN DEBRIS OR SPLATTERS THAT COULD CONTRIBUTE TO STORM WATER RUNOFF POLLUTION.
4. IF THE STREET, SIDEWALKS AND/OR PARKING LOT ARE PRESSURE WASHED, DEBRIS MUST BE TRAPPED AND COLLECTED TO PREVENT ENTRY INTO THE STORM DRAIN SYSTEM. NO CLEANING AGENT MAY BE DISCHARGED INTO THE STORM DRAIN. IF ANY CLEANING AGENT OR DEGREASER IS USED, WASHED WATER MUST BE COLLECTED AND DISCHARGED TO THE SANITARY SEWER, SUBJECT TO THE APPROVAL OF THE OWNER'S PROJECT MANAGER, OR OTHERWISE DISPOSED OF THROUGH APPROVED DISPOSAL METHODS.
5. CREATE A CONTAINED AND COVERED AREA ON THE SITE FOR THE STORAGE OF BAGS, CEMENT, PAINTS, OILS, FERTILIZERS, PESTICIDES, OR OTHER MATERIAL USED ON THE SITE THAT HAVE THE POTENTIAL OF BEING WIND-BLOWN OR IN THE EVENT OF A MATERIAL SPILL.
6. NEVER CLEAN MACHINERY, EQUIPMENT OR TOOLS INTO A STREET, GUTTER OR STORM DRAIN.
7. ENSURE THAT CEMENT TRUCKS, PAINTERS, OR STUCCO/PLASTER FINISHING CONTRACTORS DO NOT DISCHARGE WASH WATER FROM EQUIPMENT, TOOLS OR RINSE CONTAINERS INTO GUTTERS OR DRAINS.
8. THE ON-SITE STORM DRAIN FACILITIES SHALL BE CLEANED A MINIMUM OF TWICE A YEAR AS FOLLOWS: IMMEDIATELY PRIOR TO OCTOBER 15TH AND ONCE IN JANUARY. ADDITIONAL CLEANING MAY BE REQUIRED IF FOUND NECESSARY BY THE INSPECTOR. CONTRACTOR SHALL BE RESPONSIBLE FOR COST ASSOCIATED WITH CLEANING.
9. PREVENT DUST FROM LEAVING THE SITE AND ACCUMULATING ON ADJACENT AREAS AS REQUIRED IN THE DUST CONTROL NOTES ON THIS SHEET.
10. PREVENT SEDIMENT LADEN STORM RUN-OFF FROM LEAVING THE SITE OR ENTERING STORM DRAIN OR SANITARY SEWER SYSTEMS AS REQUIRED IN THE EROSION AND SEDIMENTATION CONTROL NOTES ON THIS SHEET.
11. MAINTAIN EXISTING TREES AND PLANTS THAT ARE TO REMAIN AS REQUIRED BY THE TREE AND PLANT PROTECTION NOTES ON THE SHEET.

STORMWATER POLLUTION PREVENTION NOTES:

- 1. STORE, HANDLE, AND DISPOSE OF CONSTRUCTION MATERIALS AND WASTES PROPERLY, SO AS TO PREVENT THEIR CONTACT WITH STORMWATER.
2. CONTROL AND PREVENT THE DISCHARGE OF ALL POTENTIAL POLLUTANTS, INCLUDING SOLID WASTES, PAINTS, CONCRETE, PETROLEUM PRODUCTS, CHEMICALS, WASHWATER OR SEDIMENT, AND NON-STORMWATER DISCHARGES TO STORM DRAINS AND WATER COURSES.
3. USE SEDIMENT CONTROL OR FILTRATION TO REMOVE SEDIMENT FROM DEWATERING EFFLUENT.
4. AVOID CLEANING, FUELING, OR MAINTAINING VEHICLES ON SITE, EXCEPT IN A DESIGNATED AREA IN WHICH RUNOFF IS CONTAINED AND TREATED.
5. DELINEATE CLEARING LIMITS, EASEMENTS, SETBACKS, SENSITIVE OR CRITICAL AREAS, BUFFER ZONES, TREES AND DISCHARGE COURSE WITH FIELD MARKERS.
6. PROTECT ADJACENT PROPERTIES AND UNDISTURBED AREAS FROM CONSTRUCTION IMPACTS USING VEGETATIVE BUFFER STRIPS, SEDIMENT BARRIERS OF FILTERS, DIKES, MULCHING, OR OTHER MEASURES AS APPROPRIATE.
7. PERFORM CLEARING AND EARTH MOVING ACTIVITIES DURING DRY WEATHER TO THE MAXIMUM EXTENT PRACTICAL.
8. LIMIT AND TIME APPLICATIONS OF PESTICIDES AND FERTILIZERS TO PREVENT POLLUTED RUNOFF.
9. LIMIT CONSTRUCTION ACCESS ROUTES AND STABILIZE DESIGNATED ACCESS POINTS.
10. AVOID TRACKING DIRT OR MATERIALS OFF-SITE; CLEAN OFF-SITE PAVED AREAS AND SIDEWALKS USING DRY SWEEPING METHODS TO THE MAXIMUM EXTENT PRACTICAL.

SUPPLEMENTAL MEASURES:

- A. THE PHRASE "NO DUMPING - DRAINS TO BAY" OR EQUALLY EFFECTIVE PHRASE MUST BE LABELED ON STORM DRAIN INLETS (BY STENCILING, BRANDING, OR PLAQUES) TO ALERT THE PUBLIC TO THE DESTINATION OF STORM WATER AND TO PREVENT DIRECT DISCHARGE OF POLLUTANTS INTO THE STORM DRAIN.
B. USING FILTRATION MATERIALS ON STORM DRAIN COVERS TO REMOVE SEDIMENT FROM DEWATERING EFFLUENT.
C. STABILIZING ALL DENUDEED AREAS AND MAINTAINING EROSION CONTROL MEASURES CONTINUOUSLY FROM OCTOBER 15 AND APRIL 15.
D. REMOVING SPOILS PROMPTLY, AND AVOID STOCKPILING OF FILL MATERIALS, WHEN RAIN IS FORECAST. IF RAIN THREATENS, STOCKPILED SOILS AND OTHER MATERIALS SHALL BE COVERED WITH A TARP OR OTHER WATERPROOF MATERIAL.
E. STORING, HANDLING, AND DISPOSING OF CONSTRUCTION MATERIALS AND WASTES SO AS TO AVOID THEIR ENTRY TO THE STORM DRAIN SYSTEMS OR WATER BODY.
F. AVOIDING CLEANING, FUELING, OR MAINTAINING VEHICLES ON-SITE, EXCEPT IN AN AREA DESIGNATED TO CONTAIN AND TREAT RUNOFF.
G. LIMITING AND TIMING APPLICATIONS OF PESTICIDES AND FERTILIZER TO AVOID POLLUTING RUNOFF.

WATER SYSTEM NOTES:

- 1. WHERE WATER LINES HAVE TO CROSS SANITARY SEWER LINES, DO SO AT A 90 DEGREE ANGLE AND WATER LINES SHALL BE MINIMUM OF 12" ABOVE THE TOP OF THE SANITARY SEWER LINES.
2. WATER LINES ARE SHOWN SCHEMATICALLY; CONTRACTOR SHALL IDENTIFY EACH ANGLE AND/OR BEND THAT MAY BE REQUIRED TO ACCOMPLISH THE INTENDED DESIGN.
3. USE DETECTABLE METALIZED WARNING TAPE APPROXIMATELY 6" BELOW THE SURFACE, TAPE SHALL BE A BRIGHT COLOR AND IMPRINTED WITH "CAUTION-WATER LINE BELOW", CALPICO TYPE 2 OR EQUAL.
4. ALL WATER SERVICE CONNECTIONS SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OR APPLICABLE WATER DISTRICT STANDARDS.
5. PUBLIC AND PRIVATE WATER MAIN AND WATER SERVICE LINE 4-INCH THROUGH 12-INCH SHALL BE POLYVINYL CHLORIDE (PVC) AND SHALL MEET AWWA C900, RATED FOR 200 PSI GLASS PIPE WITH EPOXY COATED DUCTILE IRON FITTINGS AND FUSION EPOXY COATED GATE VALVES. ALL JOINTS SHALL FACTORY MANUFACTURED WITH BELL AND SPIGOT ENDS AND RUBBER GASKETS. NONMETALLIC WATER LINES HAVE TRACER WIRE INSTALLED.
6. CONNECTION TO THE EXISTING WATER MAIN SHALL BE APPROVED BY WATER COMPANY. THE DISTRICT SHALL PAY THE ACTUAL COSTS OF CONSTRUCTION. THE CONTRACTOR SHALL PERFORM ALL EXCAVATION PREPARE THE SITE, FURNISH ALL MATERIALS, INSTALL TAPPING TEE VALVE AND ALL THRUST BLOCKS. BACKFILL, RESTORE THE SURFACE, AND CLEANUP. ALL WET TAPS SHALL BE APPROVED BY THE CITY OR APPLICABLE WATER DISTRICT. NONMETALLIC WATER LINES SHALL HAVE TRACER WIRES INSTALLED.
7. ALL WATER LINES 3" OR SMALLER SHALL BE TYPE K COPPER WITH SILVER BRAZED JOINTS. POLYETHYLENE PIPE MAY BE SUBSTITUTED, CONTRACTOR SHOULD SEEK APPROVAL FROM DISTRICT BEFORE MAKING SUBSTITUTION. CONTRACTOR TO VERIFY PRESSURES FROM EXISTING LINES ARE ADEQUATE TO SERVICE BUILDINGS AS SPECIFIED BY THE PLUMBING PLANS.
8. ALL WATER LINES SHALL BE INSTALLED WITH 3' MINIMUM COVER.
9. ALL WATER VALVES SHALL BE PER CITY STANDARD.
10. ALL TEMPORARY AND/OR PERMANENT AIR-RELEASE AND BLOW-OFF VALVES SHALL BE PER CITY STANDARD AND AS DIRECTED BY THE CITY ENGINEER.
11. CONCRETE THRUST BLOCKS SHALL BE INSTALLED AT ALL TEES, CROSSINGS, BENDS (HORIZONTAL AND VERTICAL), AT SIZE CHANGES AND AT FIRE HYDRANTS PER CITY STANDARD. AWWA C800, SECTION 3.8 UNLESS NOTED OTHERWISE.
12. MECHANICALLY RESTRAINED JOINTS SHALL BE INSTALLED AT VERTICAL BENDS IN ACCORDANCE WITH CITY STANDARDS AND AS APPROVED BY THE CITY ENGINEER.
13. ALL WATER VALVES SHALL BE CLUSTERED, UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER.

STORM DRAIN NOTES:

- 1. ALL STORM DRAIN PIPE SHALL BE PVC PER SECTION 02630, SLOPED AT 2% UNLESS OTHERWISE SPECIFIED ON THE PLANS. PIPE SHALL BE SIZED AS SPECIFIED ON THE PLANS. ALL DIRECTION CHANGES SHALL BE MADE WITH A Y CONNECTION OR LONG SWEEP ELBOWS, REGULAR ELBOWS, AND TEE'S SHOULD BE AVOIDED.
2. USE DETECTABLE METALIZED WARNING TAPE APPROXIMATE 6" BELOW THE SURFACE. TAPE SHALL BE A BRIGHT COLOR AND IMPRINTED WITH "CAUTION- STORM DRAIN LINE BELOW", CALPICO TYPE 2 OR EQUAL.
3. PAINT THE TOP OF THE CURBS ADJACENT TO EACH CATCH BASIN INSTALLED UNDER THE WORK OR ADJACENT TO THIS SITE WITH THE WORDS "NO DUMPING". WORDING TO BE BLUE 4" HIGH LETTERS ON A PAINTED WHITE BACKGROUND. A " NO DUMPING"
4. ALL AREA DRAINS AND CATCH BASINS GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS AND HAVE BOLT DOWN GRATES.
5. ALL TRENCHES SHALL BE BACKFILLED PER THE SPECIFICATIONS OF THE CIVIL ENGINEER TO VERIFY COMPACTION VALUES.
6. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POTHOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR CROSSED PRIOR TO TRENCH OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
7. COMPLETE SYSTEMS; ALL UTILITY SYSTEMS ARE DELINEATED IN SCHEMATIC MANNER ON THESE PLANS. CONTRACTOR IS TO PROVIDE ALL FITTINGS, ACCESSORIES, AND WORK NECESSARY TO COMPLETE THE UTILITY SYSTEM SO THAT IT IS FULLY FUNCTIONING FOR THE PURPOSE INTENDED.

SANITARY SEWER NOTES:

- 1. INSTALL DETECTABLE METALIZED WARNING TAPE APPROXIMATELY 6"-12" BELOW THE SURFACE IN NON-PAVED AREAS, AND AT THE BOTTOM OF BASEROCK FOR PAVED AREAS. GREEN IMPRINTED WITH "CAUTION-SANITARY SEWER LINE BELOW", CALPICO TYPE 2 OR EQUAL.
2. ALL SEWER WORK SHALL BE IN CONFORMANCE WITH THE CITY OR APPROPRIATE SANITARY SEWER DISTRICT.
3. PUBLIC AND PRIVATE SANITARY SEWER MAIN AND SERVICE LINE 4-INCH THROUGH 8-INCH SHALL BE POLYVINYL CHLORIDE (PVC) SDR 26 GREEN SEWER PIPE AND SHALL CONFORM TO THE REQUIREMENTS OF ASTM DESIGNATION D 3034-08 WITH GLUED JOINTS.

DEMOLITION NOTES:

- 1. CONTRACTOR IS TO COMPLY WITH ALL GENERAL AND STATE REQUIREMENTS INVOLVING THE REMOVAL AND DISPOSAL OF HAZARDOUS MATERIAL(S).
2. THE CONTRACTOR SHALL LOCATE AND CLEARLY MARK (AND THEN PRESERVE THESE MARKERS) FOR THE DURATION OF CONSTRUCTION OF ALL TELEPHONE, DATA, STREET LIGHT, SIGNAL LIGHT AND POWER FACILITIES THAT ARE IN OR NEAR THE AREA OF CONSTRUCTION.
3. CONTRACTOR'S BID IS TO INCLUDE ALL VISIBLE SURFACE AND ALL SUBSURFACE FEATURES IDENTIFIED TO BE REMOVED OR ABANDONED IN THESE DOCUMENTS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR A SITE INSPECTION TO FULLY ACKNOWLEDGE THE EXTENT OF THE DEMOLITION WORK.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY AND ALL PERMITS NECESSARY FOR ENCROACHMENT, GRADING, DEMOLITION, AND STATE JURISDICTIONS. THE CONTRACTOR SHALL PAY ALL FEES ASSOCIATED CONTRACTOR SHALL PAY DISPOSAL FEES.
6. CONTRACTOR SHALL PAY DISPOSAL FEES.
7. BACKFILL ALL DEPRESSIONS AND TRENCHES FROM DEMOLITION OF FOUNDATIONS & UTILITIES.
8. WITHIN LIMITS OF WORK, REMOVE CURBS, GUTTERS, LANDSCAPING, SIGNAGE, TREES, SCRUBS, ASPHALT, UNDERGROUND PIPES, ETC. AS INDICATED ON THE PLANS AND SPECS.
9. REMOVAL OF LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIALS.
10. PRIOR TO BEGINNING DEMOLITION WORK ACTIVITIES, CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES OUTLINED IN THE EROSION & SEDIMENTATION CONTROL PLAN & DETAILS.
11. CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSING ALL DEMOLITION MATERIALS, OR STORING SELECTED ITEMS BY OWNER'S REPRESENTATIVE AT DESIGNATED LOCATIONS.
12. THE CONTRACTOR SHALL MAINTAIN ALL SAFETY DEVICES, AND SHALL BE RESPONSIBLE FOR CONFORMANCE TO ALL LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS LAWS AND REGULATIONS.
13. THE CONTRACTOR SHALL PROTECT FROM DAMAGE ALL EXISTING IMPROVEMENTS FACILITIES AND STRUCTURES WHICH ARE TO REMAIN. ANY ITEMS DAMAGED BY THE CONTRACTOR OR HIS AGENTS OF ANY ITEMS REMOVED FOR HIS USE SHALL BE REPLACED IN EQUAL OR BETTER CONDITION AS APPROVED BY THE ARCHITECT OR OWNER'S REPRESENTATIVE.
14. COORDINATE WITH ELECTRICAL, MECHANICAL, FIRE PROTECTION AND ARCHITECTURAL DRAWINGS FOR UTILITY SHUT-DOWN / DISCONNECT LOCATIONS. CONTRACTOR IS TO SHUT OFF ALL UTILITIES AS NECESSARY PRIOR TO DEMOLITION. CONTRACTOR IS TO COORDINATE SERVICE INTERRUPTIONS WITH THE OWNER. DO NOT INTERRUPT SERVICES ADJACENT OFF-SITE OWNERS. ALSO SEE ARCHITECTURAL PLANS FOR ADDITIONAL SCOPE OF WORK.
15. DEMOLITION INCLUDES REMOVAL OF ALL ITEMS ASSOCIATED WITH THE UTILITIES AND SHALL INCLUDE PREPARING THE SITE FOR NEW UTILITIES, BUILDINGS, RETAINING WALLS, ETC.
16. ALL MATERIALS TO BE DEMOLISHED AND REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE LAWFULLY DISPOSED OF OFF-SITE.
17. THE PLAN IS NOT INTENDED TO BE A COMPLETE CATALOGUE OF ALL EXISTING STRUCTURES AND UTILITIES. THIS PLAN INTENDS TO DISCLOSE GENERAL INFORMATION KNOWN BY THE ENGINEER AND TO SHOW THE LIMITS OF THE AREA WHERE WORK WILL BE PERFORMED. THIS PLAN SHOWS THE EXISTING FEATURES TAKEN FROM A FIELD SURVEY, FIELD INVESTIGATIONS AND AVAILABLE INFORMATION. THIS PLAN MAY OR MAY NOT ACCURATELY REFLECT THE TYPE OR EXTENT OF THE ITEMS TO BE ENCOUNTERED AS THEY ACTUALLY EXIST. WHERE EXISTING FEATURES ARE NOT SHOWN, IT IS IMPLIED THAT THEY ARE NOT TO BE DEMOLISHED OR REMOVED. THE CONTRACTOR SHALL PERFORM A THOROUGH FIELD INVESTIGATION AND REVIEW OF THE SITE WITHIN THE LIMIT OF WORK SHOWN IN THIS PLAN SET TO DETERMINE THE TYPE, QUANTITY AND EXTENT OF ANY AND ALL ITEMS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THE EXTENT OF EXISTING STRUCTURES AND UTILITIES AND QUANTITY OR WORK INVOLVED IN REMOVING THESE ITEMS FROM THE SITE.



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GAS STATION AND CAR WASH EXPANSION AND UPGRADE 438 WEST TENNYSON RD. HAYWARD, CALIFORNIA ALAMEDA COUNTY APN: 465-0001-002

GRADING SPECIFICATIONS

Table with 2 columns: REVISIONS, BY. Includes fields for JOB NO: 216026, DATE: 3-21-17, SCALE: AS NOTED, DESIGN BY: WCC, DRAWN BY: DR, SHEET NO.



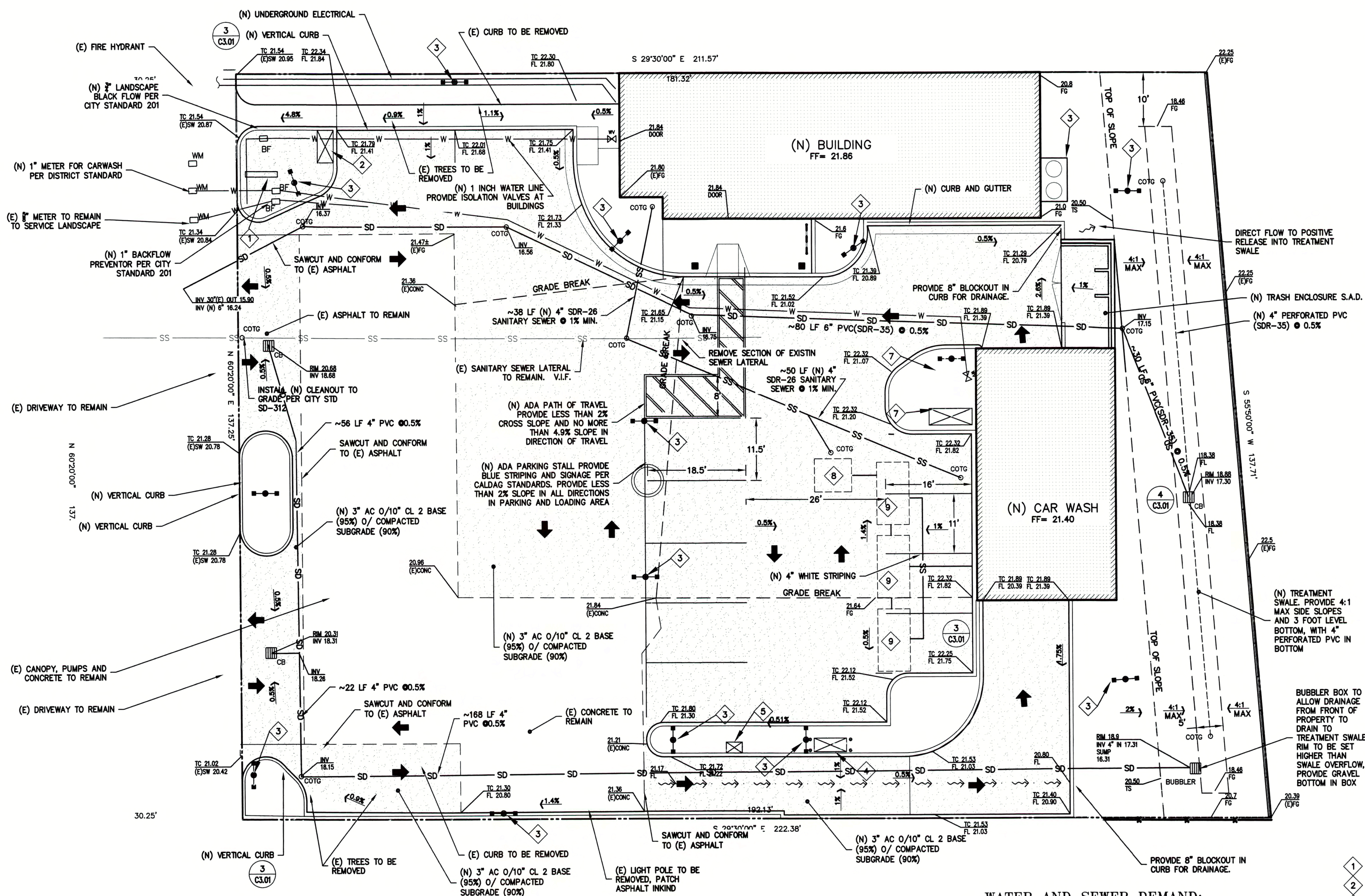
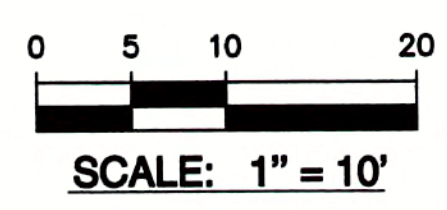


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**GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD.
HAYWARD, CALIFORNIA**
ALAMEDA COUNTY APRN: 465-0001-002

GRADING PLAN



GRADING NOTES:

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IDENTIFY, LOCATE AND PROTECT ALL UNDERGROUND FACILITIES. UNDERGROUND FACILITIES DAMAGED DURING GRADING SHALL BE REPAIRED AND/OR REPLACED TO LIKE NEW CONDITION AT NO ADDITIONAL COST TO CONTRACTOR. REFER TO TOPOGRAPHIC SURVEY AND UTILITY SURVEY FOR ADDITIONAL INFORMATION.

THE CONTRACTOR SHALL MAINTAIN THE STREETS, SIDEWALKS AND ALL OTHER PUBLIC RIGHT-OF-WAY IN A CLEAN, SAFE AND USABLE CONDITION. ALL SPILLS OF SOIL, ROCK OR CONSTRUCTION DEBRIS SHALL BE REMOVED FROM THE PUBLICLY OWNED PROPERTY DURING CONSTRUCTION AND UPON COMPLETION OF THE PROJECT. ALL ADJACENT PROPERTY, PRIVATE OR PUBLIC SHALL BE MAINTAINED IN A CLEAN, SAFE AND USABLE CONDITION.

ALL GRADING SHALL BE PERFORMED IN SUCH A MANNER AS TO COMPLY WITH THE STANDARDS ESTABLISHED BY THE AIR QUALITY MANAGEMENT DISTRICT FOR AIRBORNE PARTICULATES.

IN THE EVENT THAT HUMAN REMAINS AND/OR CULTURAL MATERIALS ARE FOUND, ALL PROJECT-RELATED CONSTRUCTION SHOULD CEASE WITHIN A 100-FOOT RADIUS. THE CONTRACTOR SHALL, PURSUANT TO SECTION 7050.5 OF THE HEALTH AND SAFETY CODE, AND SECTION 5097.94 OF THE PUBLIC RESOURCES CODE OF THE STATE OF CALIFORNIA, NOTIFY THE CORONER IMMEDIATELY.

CONTRACTOR VERIFY EXISTING UTILITY STUB LOCATIONS AND DEPTHS PRIOR TO COMMENCING CONSTRUCTION.

FINISHED GRADES SHALL BE SLOPED TOWARD INLETS OR POSITIVE RELEASE AT 0.5% MIN. FOR CONCRETE AND 1% MIN FOR ASPHALT AREAS.

REFER TO ARCHITECTURAL AND/OR LANDSCAPE PLANS FOR ADDITIONAL INFORMATION ON FLAT WORK, PAVING TYPE AND SCORING.

REFER TO ARCHITECTURAL PLANS FOR ACCESSIBLE PATH OF TRAVEL. GRADES SHALL BE DONE PER FEDERAL AND STATE ACCESSIBILITY REQUIREMENTS. IF CONTRACTOR BECOMES AWARE OF GRADES THAT ARE NOT CONFORMING TO ACCESSIBILITY REQUIREMENTS, HE SHALL BRING THIS TO THE ATTENTION OF THE ARCHITECT AND THE ENGINEER.

CUT AND FILL SLOPES AND GRADING TRANSITIONS AT THE OUTER EDGES OF THE PROPOSED IMPROVEMENTS ARE TO BE CONSTRUCTED AT THREE HORIZONTAL TO ONE VERTICAL (3:1) UNLESS OTHERWISE NOTED.

UTILITY NOTES:

STORM DRAIN PIPING SHALL BE PVC SDR-35 OR BETTER OR DOUBLE WALLED HDPE PIPING ADS N-12 OR APPROVED EQUAL. 6" MIN U.O.N.

CONTRACTOR SHALL VERIFY BUILDING CONNECTIONS AND ELEVATION. THIS INCLUDES RAIN WATER LEADER, SEWER CONNECTION AND WATER CONNECTION. NOTIFY ENGINEER OF ANY CONFLICTS.

ALL BUILDING POINTS OF CONNECTION SHALL BE VERIFIED WITH BUILDING PLUMBING DRAWING. NOTIFY ENGINEER OF ANY CONFLICT.

CONTRACTOR SHALL VERIFY EXISTING GRADES FOR ACCURACY PRIOR TO THE STARTING OF GRADING. NOTIFY THE ENGINEER IMMEDIATELY SHOULD CONFLICTS ARISE AND REDIRECT WORK TO AVOID DELAY.

THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING UTILITIES, STRUCTURES AND SERVICES BEFORE COMMENCING WORK. THE LOCATIONS OF UTILITIES STRUCTURES AND SERVICES SHOWN IN THE CONTRACT DOCUMENTS SHALL BE DEEMED TO BE APPROXIMATIONS ONLY. ALL DISCREPANCIES BETWEEN WHAT IS SHOWN AND THE ACTUAL FIELD CONDITIONS SHALL BE REPORTED TO THE DISTRICT REPRESENTATIVE. THE CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT (USA) AT (800) 227-2600 PRIOR TO ANY DEMOLITION OR EXCAVATION. UPON COMPLETION OF USA MARKING OPERATIONS, CONTRACTOR SHALL RECORD ALL UTILITY MARKINGS ON A SEPARATE SET OF DRAWINGS. THIS SET SHALL BE KEPT ON-SITE FOR REFERENCE FOR DURATION OF CONTRACT.

PROPOSED GRADES SHALL MEET EXISTING GRADES WITH A SMOOTH AND CONTINUOUS TRANSITION SO AS TO AVOID TRAPPING WATER. CONTRACTOR SHALL NOTIFY DISTRICT REPRESENTATIVE IF PUDDLING IS SUSPECTED AND REDIRECT WORK SO AS TO AVOID DELAY WHILE AWAITING RESPONSE.

ALL EXISTING DRAINAGE STRUCTURES, BOXES, UTILITY VAULTS ETC. SHALL BE BROUGHT TO FINAL FINISH GRADE PRIOR TO FINAL SURFACE TREATMENT, UNLESS NOTED OTHERWISE.

THE CONTRACTOR IS TO ENSURE THAT ALL REMAINING ACTIVE AND NEW DRAINAGE AND UTILITY LINES ARE PROTECTED AND UNDAMAGED FROM TRENCHING AND FOOTING EXCAVATIONS.

CONTRACTOR IS TO ENSURE THAT ALL AREAS ARE GRADED TO PROVIDE POSITIVE DRAINAGE TO IDENTIFIED EXISTING AND PROPOSED DRAIN INLETS.

AREAS OF TRENCHING SHALL BE PATCHED TO MATCH EXISTING CONDITIONS TO LIKE NEW CONDITIONS, INCLUDING BUT NOT LIMITED TO SOD, CONCRETE AND ASPHALT.

WATER MAINS AND SERVICES SHALL BE LOCATED 10 FEET HORIZONTALLY FROM AND ONE FOOT VERTICALLY ABOVE PARALLEL SEWER LINES AND 4 FEET AND ONE FOOT FOR STORM DRAIN LINES.

ANNOTATION NOTES

- 1 (N) SIGN SEE ARCHITECTURAL PLANS
- 2 (N) AIR AND WATER STATION S.A.D.
- 3 (N) LIGHT STANDARD S.A.D.
- 4 GAS RELEASE TANK S.A.D.
- 5 CAR WASH CONTROL CONSOLE
- 6 FREEZER COMPRESSOR PAD
- 7 VACUUM BOOSTER, S.A.D.
- 8 750 GALLON OIL WATER SEPERATOR, BY OTHERS.
- 9 2000 GALLON UNDERGROUND WATER RECYCLING SYSTEM TANKS BY OTHERS

WATER AND SEWER DEMAND:

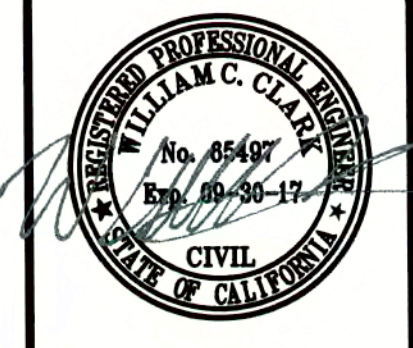
USAGE ASSUMPTIONS:
WASHES PER DAY
TOTAL GALLONS USED PER WASH = 75
% OF WATER USED FROM RECLAIM = 75%
FRESH WATER USED PER WASH = 12 GALLON
TOTAL FRESH WATER PER DAY = 900
TOTAL DISCHARGE TO SEWER PER DAY = 450-600 GALLONS
(WITH ALLOWANCE FOR WATER DRIPPING OFF CARS AND EVAPORATION AT 3-6 GALLONS PER CAR)

MINIMUM WATER SUPPLY IS 38 GPM AT 30 PSI.

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SHEET NO:





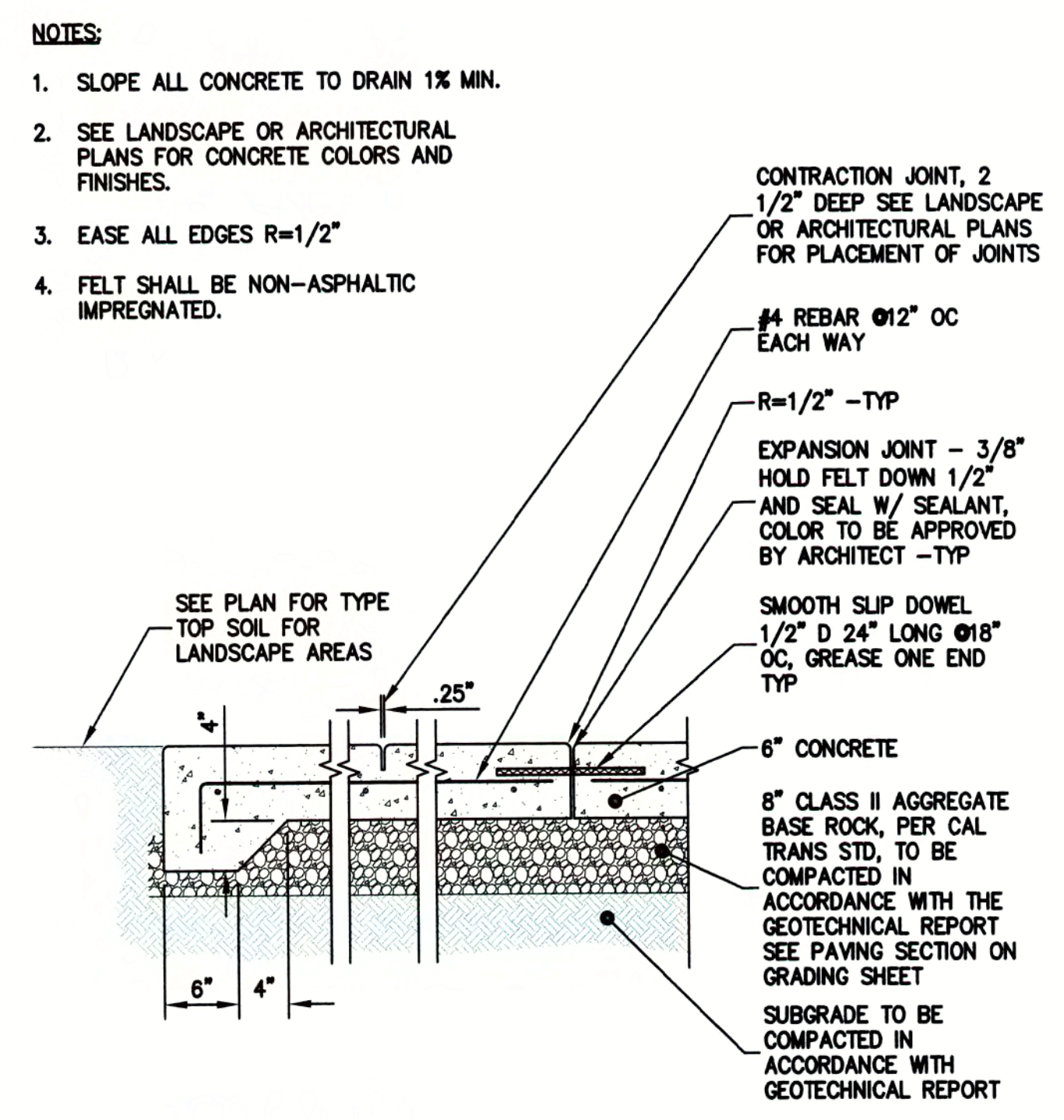
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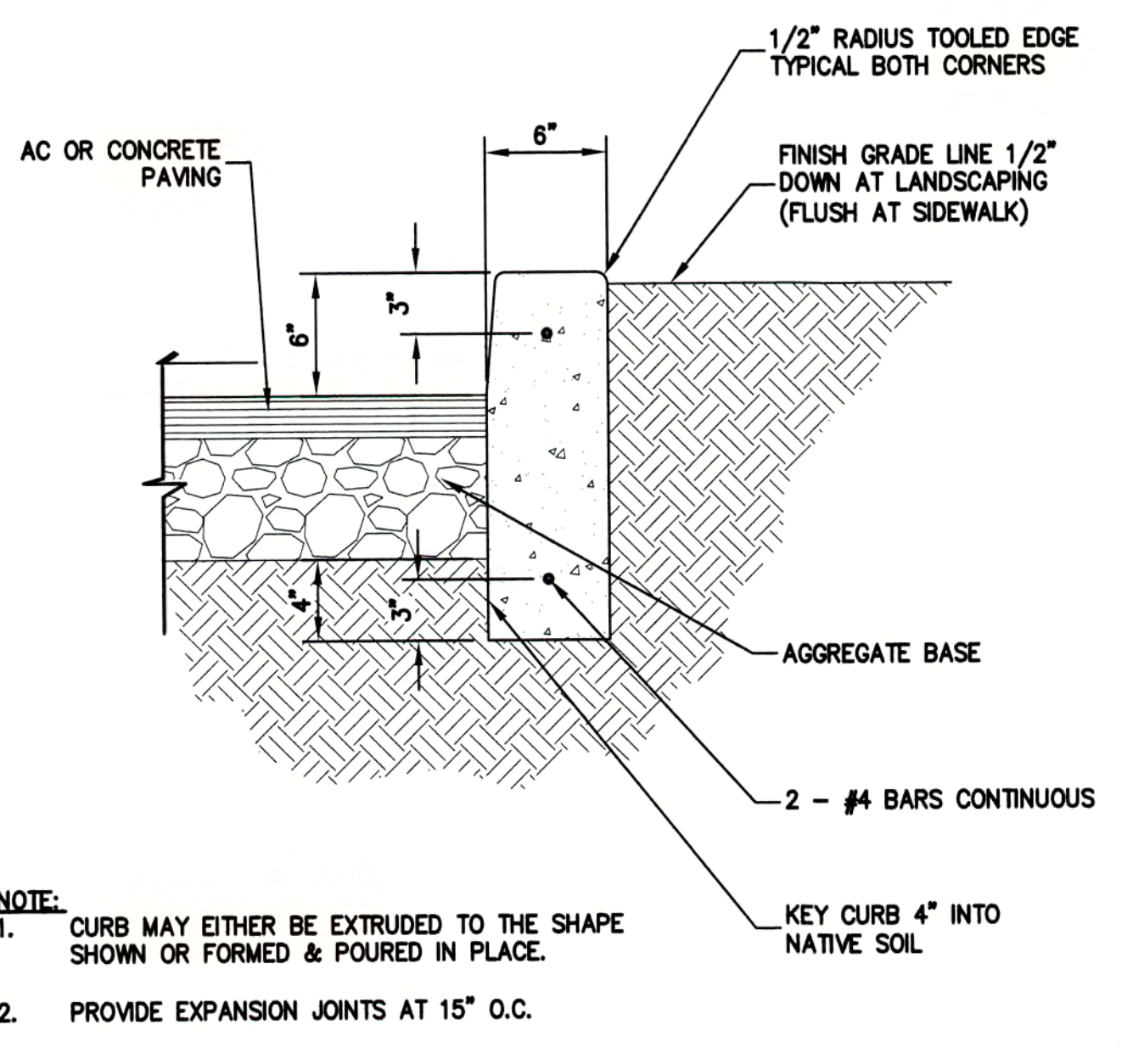
DETAILS

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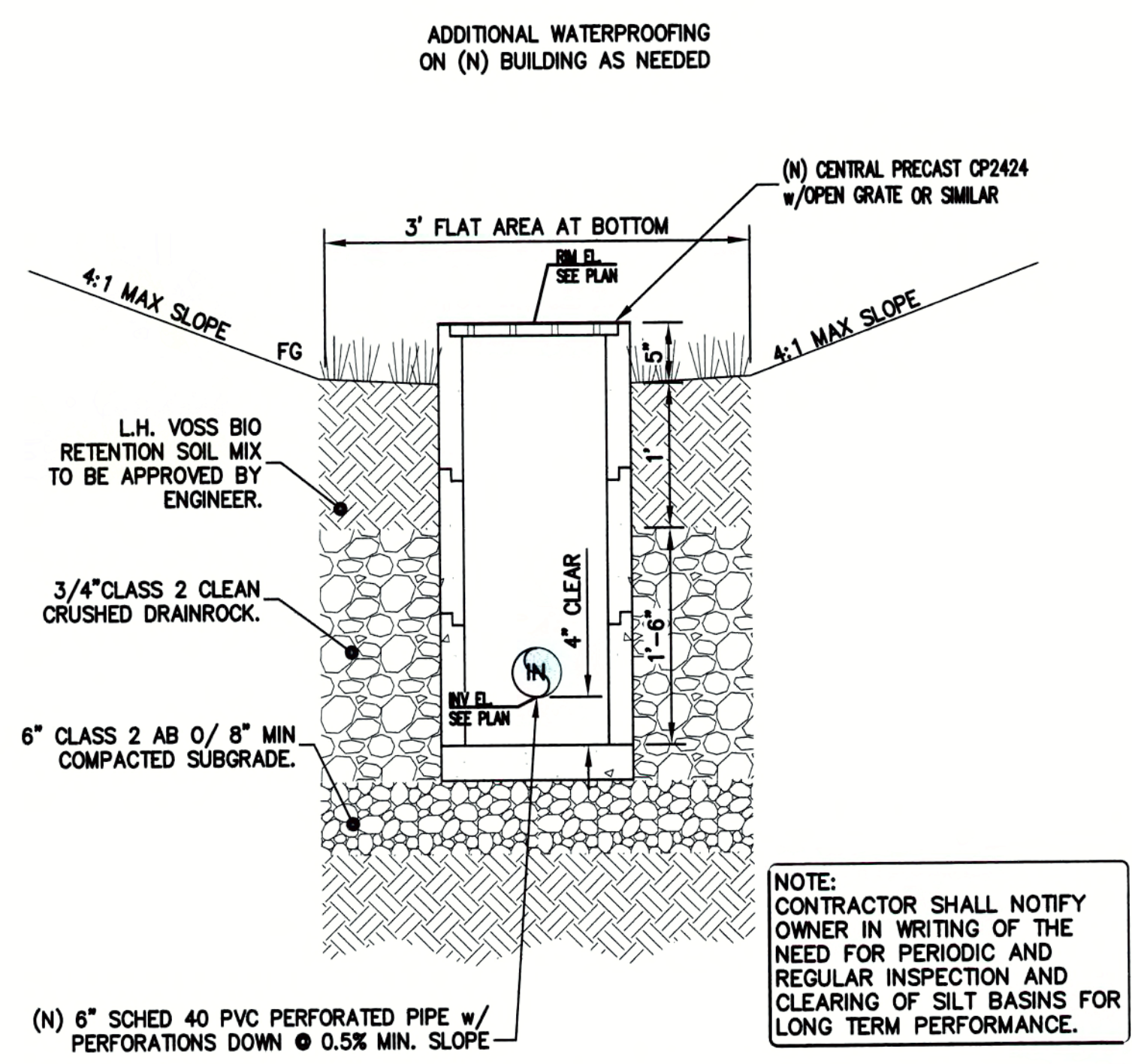
C3.1



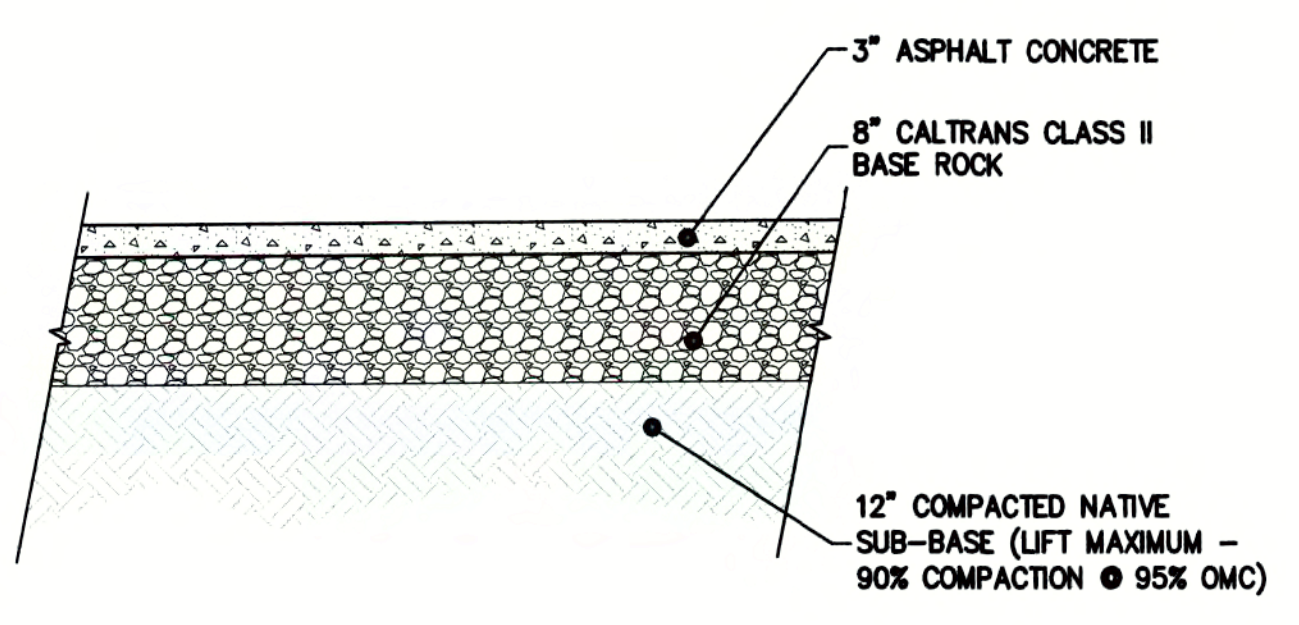
2 CONCRETE PAVING
 C3.1 NTS



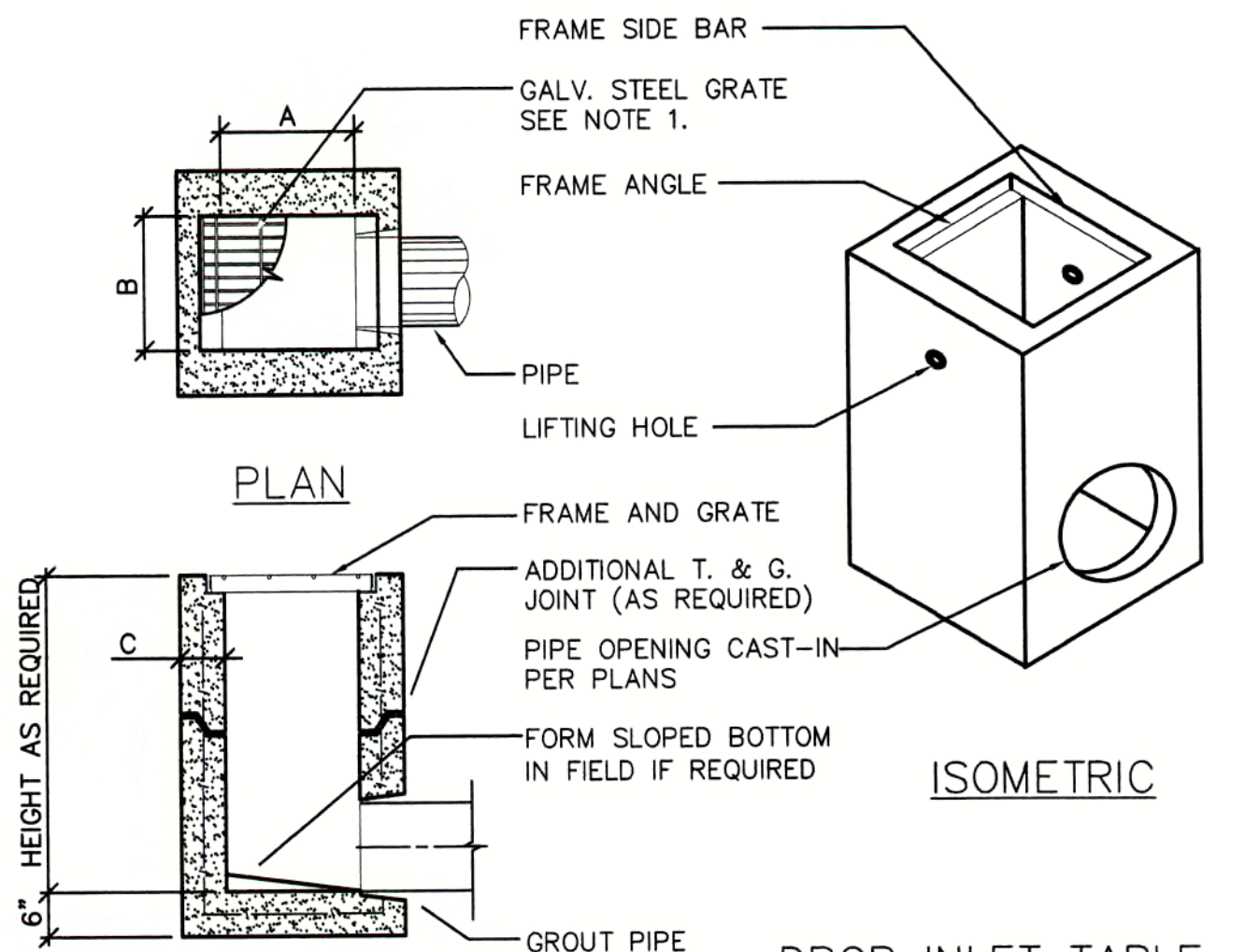
3 TYPICAL CURB
 C3.1 NTS



4 BIO-SWALE & CATCH BASIN
 C3.1 NTS



1 ASPHALT SECTION
 C3.1 NTS



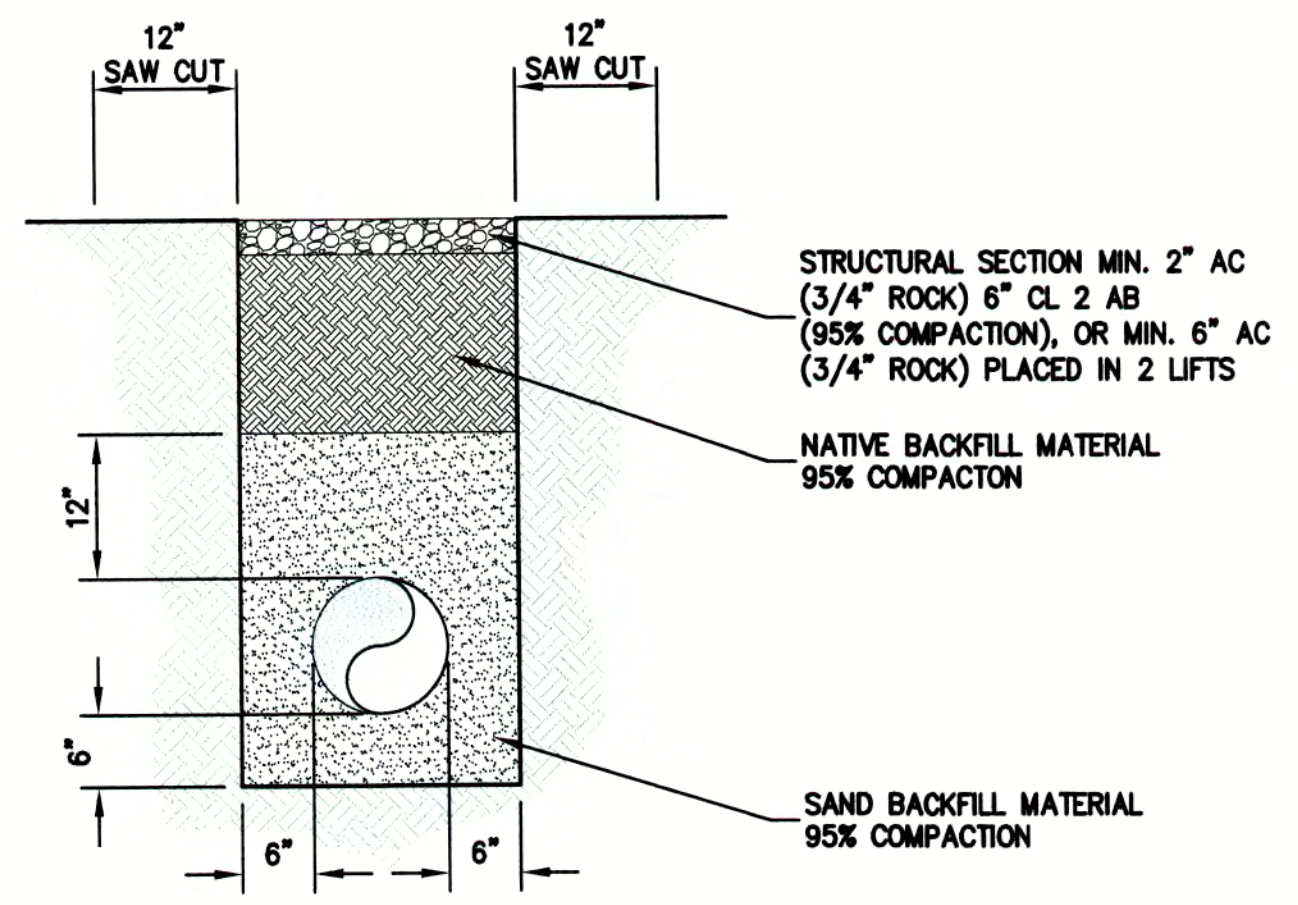
DROP INLET TABLE

MODEL No.	CPC MODEL NAME	A IN	B IN	C IN
CP1212	EK	12	300	4
CP1818	CK	18	450	5
CP1824	1K*	18	450	5
CP2424	2K	24	600	5
CP2430	3K	24	600	5
CP3030	5K	30	750	6
CP2436	1L	24	600	6
CP3636	1M	36	900	6
CP2448	3L	24	600	6
CP3648	3M	36	900	6
CP4848	1R	48	1200	6

TYPICAL SECTION

- NOTES :**
- FRAMES AND GRATES MAY BE SPECIFIED FOR PEDESTRIAN OR H2O TRAFFIC LOADING. ALL GRATES ARE BICYCLE PROOF. OPTIONAL GRATE LOCKING DEVICE AVAILABLE ON REQUEST SEE DRAWING "LOCK" ON PAGE 1-7. CLOSED-MESH GRATES OR CAST IRON FRAME AND GRATES ARE AVAILABLE ON REQUEST.
 - FOR SURFACE AND DISCHARGE OPTIONS AVAILABLE SEE DRAWING NO. "DI-SO" PAGE 1-9 AND "DI-DO" PAGE 1-5.
 - FRAMES AND GRATES DETAILS SEE PAGES 1-8, 1-9, AND 1-10.
 - WALL THICKNESSES ON ALL D.I.S. CAN BE CHANGED UPON REQUEST.
 - 18" WIDE D.I.'S REPLACE THE OLD 16" WIDE BOX BK & 1K.

6 CATCH BASIN DETAIL
 C3.2 NTS



5 TRENCH BACKFILL
 C3.1 NTS



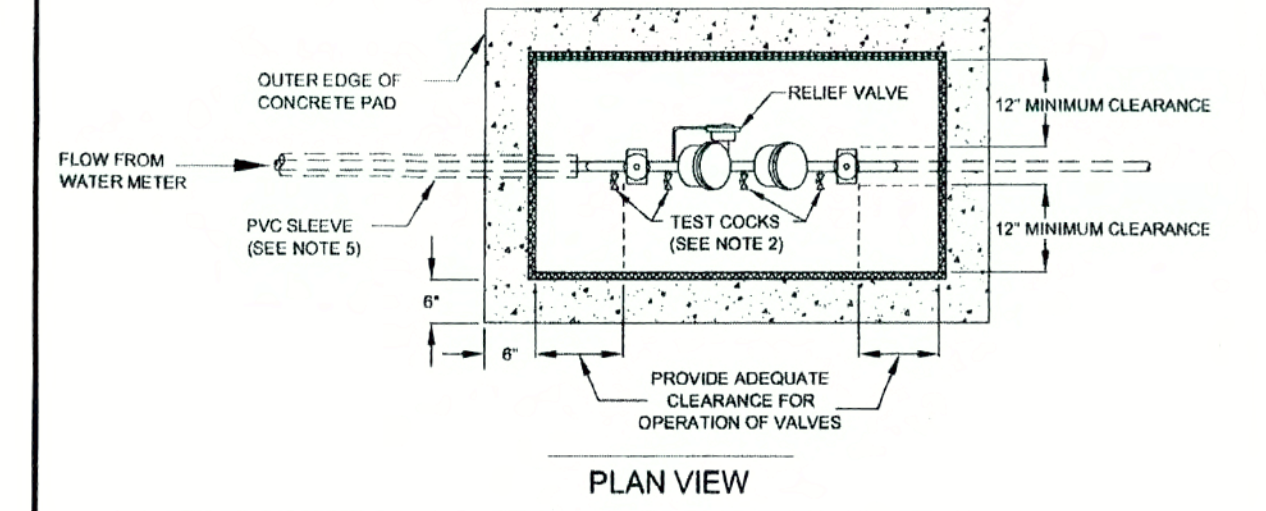
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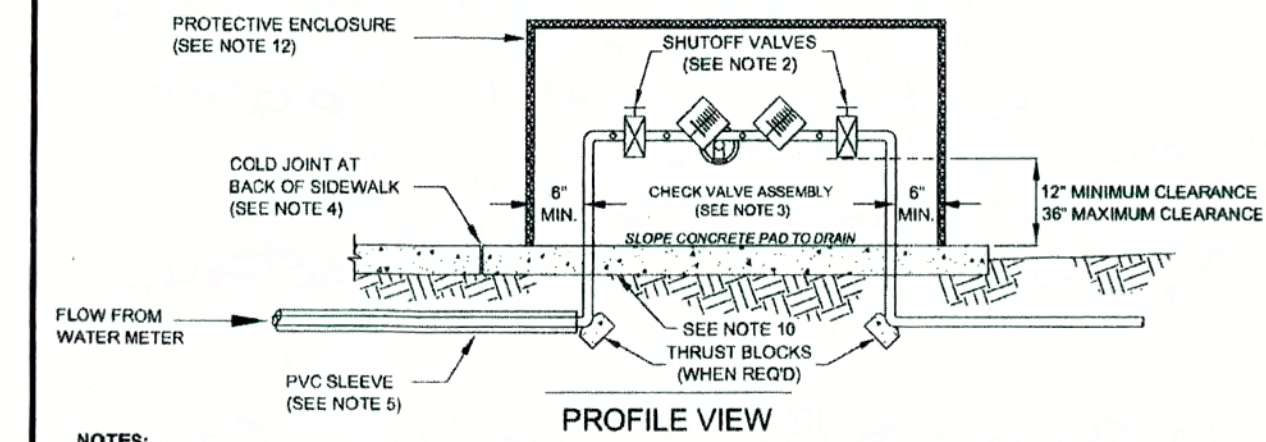
GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD.
HAYWARD, CALIFORNIA
ALAMEDA COUNTY
APN: 465-0001-002

DETAILS

FOR IRRIGATION SERVICES AND WATER SERVICES
WHERE REQUIRED BY CODE



PLAN VIEW



PROFILE VIEW

- NOTES:
1. REDUCED PRESSURE PRINCIPAL BACKFLOW PREVENTION ASSEMBLIES (RPP) AND REDUCED PRESSURE PRINCIPAL DETECTOR ASSEMBLIES (RPDA) SHALL CONFORM TO THE REQUIREMENTS OF AWWA C511, THE CALIFORNIA CODE OF REGULATIONS TITLE 17, AND SHALL BE APPROVED BY THE CALIFORNIA DEPARTMENT OF PUBLIC HEALTH.
 2. ALL SHUT-OFF VALVES SHALL BE RESILIENT-SEATED. FOR VALVES 2" OR LARGER, THEY SHALL ALSO CONFORM TO AWWA C509 AND INCLUDE PIPE SUPPORTS. TEST COCKS SHALL BE RESILIENT-SEATED FULL-PORTED.
 3. THE RPP SHOWN ABOVE REPRESENTS A TYPICAL SCHEMATIC. THE ACTUAL APPEARANCE OF AN APPROVED DEVICE MAY VARY.
 4. BACKFLOW DEVICE SHALL BE LOCATED ON A CONCRETE PAD (A) WITHIN 8 FEET FROM BACK OF SIDEWALK (B) A MAXIMUM OF 30" FROM THE BACK OF SIDEWALK WHERE SCREENING IS REQUIRED, OR (C) AT A LOCATION DETERMINED BY THE CROSS CONNECTION PERSONNEL IN THE FIELD.
 5. THE SERVICE LINE FROM BACK OF METER TO THE DEVICE SHALL BE INSTALLED IN A MINIMUM 3" Ø PVC CASING/SLEEVE.
 6. THE PORTION OF THE TRENCH FROM BACK OF METER TO THE DEVICE SHALL REMAIN OPEN UNTIL WATER DISTRIBUTION CROSS CONNECTION PERSONNEL HAVE INSPECTED AND APPROVED THE INSTALLATION.
 7. AT LOCATIONS WITH MULTIPLE DEVICES, THESE DEVICES SHALL HAVE A MINIMUM CLEARANCE OF 18" BETWEEN THEM.
 8. THE TESTING SIDE OF THE DEVICE SHALL HAVE 24" MINIMUM CLEARANCE FROM OBSTRUCTIONS (BUILDINGS, NON-FRANABLE LANDSCAPING, UTILITIES, ETC.) OTHER THAN ADJACENT BACKFLOW DEVICES.
 9. ALL METAL EXPOSED TO SOIL OR CONCRETE SHALL BE EVENLY COATED WITH 3M SCOTCHWRAP PIPE PRIMER AND THEN WRAPPED WITH 3M SCOTCHWRAP NO. 55 BLACK PVC TAPE (8" OVERLAP).
 10. CONCRETE PAD SHALL BE CLASS B CONCRETE, 4" MINIMUM THICKNESS.
 11. PROTECTIVE ENCLOSURE SHALL BE A HINGED & LOCKABLE STEEL WIRE CAGE, POWDER COATED GREEN.
 12. VALVE ASSEMBLY SHALL BE COVERED WITH AN INSULATION BLANKET, MINIMUM R-13, GREEN, WEATHERGUARD OR EQUAL.
 13. FOR VALVES 2" AND LARGER, HANDWHEELS SHALL BE CHAINED AND LOCKED TO PREVENT UNAUTHORIZED OPERATION.
 14. BOLLARDS SHALL BE INSTALLED AS DETERMINED BY THE CITY ENGINEER.

		STANDARD - REDUCED PRESSURE PRINCIPAL BACKFLOW PREVENTION ASSEMBLIES	DWG. NO. SD-202 FILED 1 OF 1
REV.	DATE	BY	
1	4/14/10	JK	

- NOTES:
1. The water service piping shall be run in a straight line perpendicular to the curb from main to meter location.
 2. All connections to copper tubing shall be flared.
 3. Water Department only will install meter.

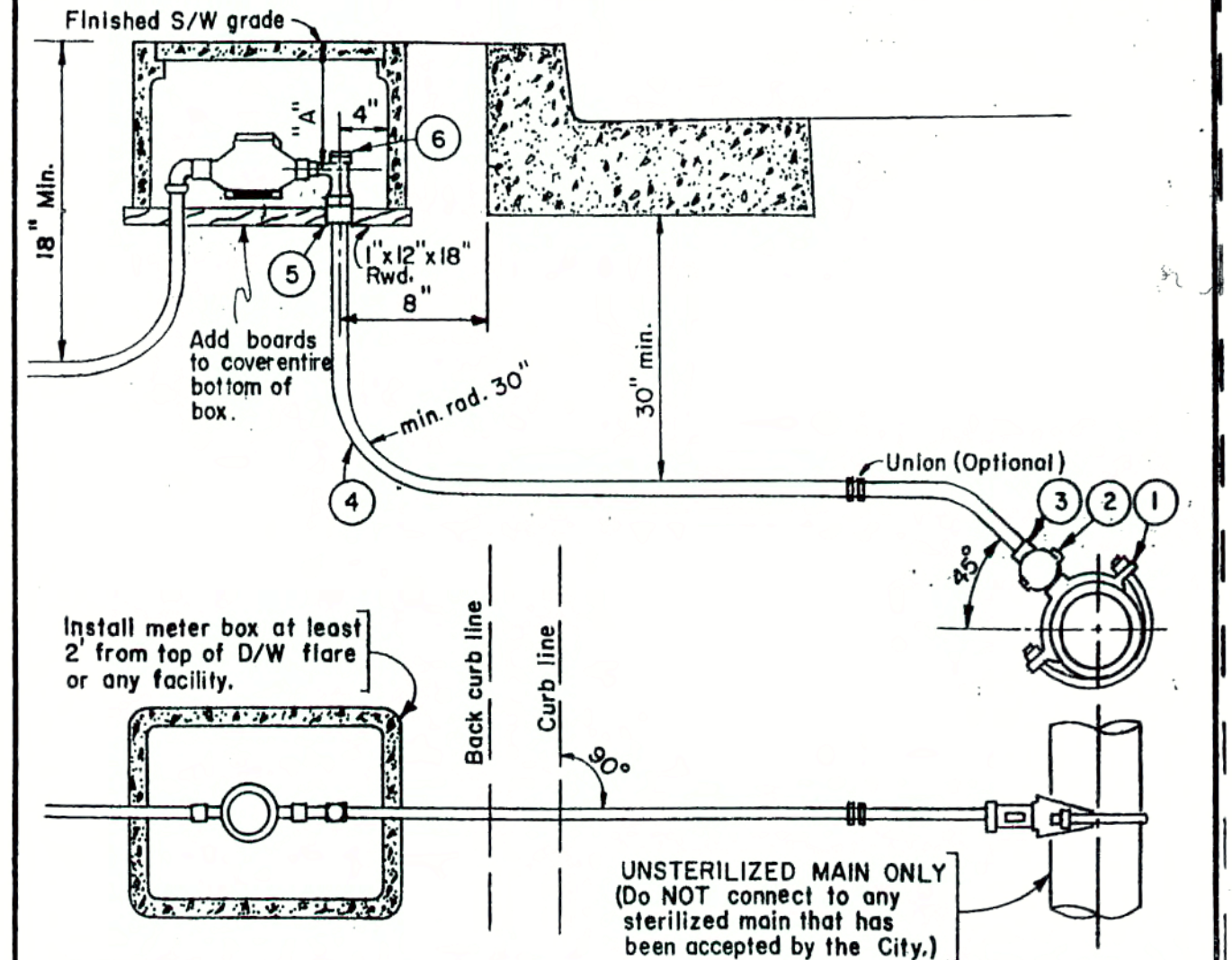
SADDLES REQUIRED FOR
CORP. STOP TAP

Water main Size	Type	A	A
3"	4"		
4"	D.I.	A	A
larger	PVC	B	B

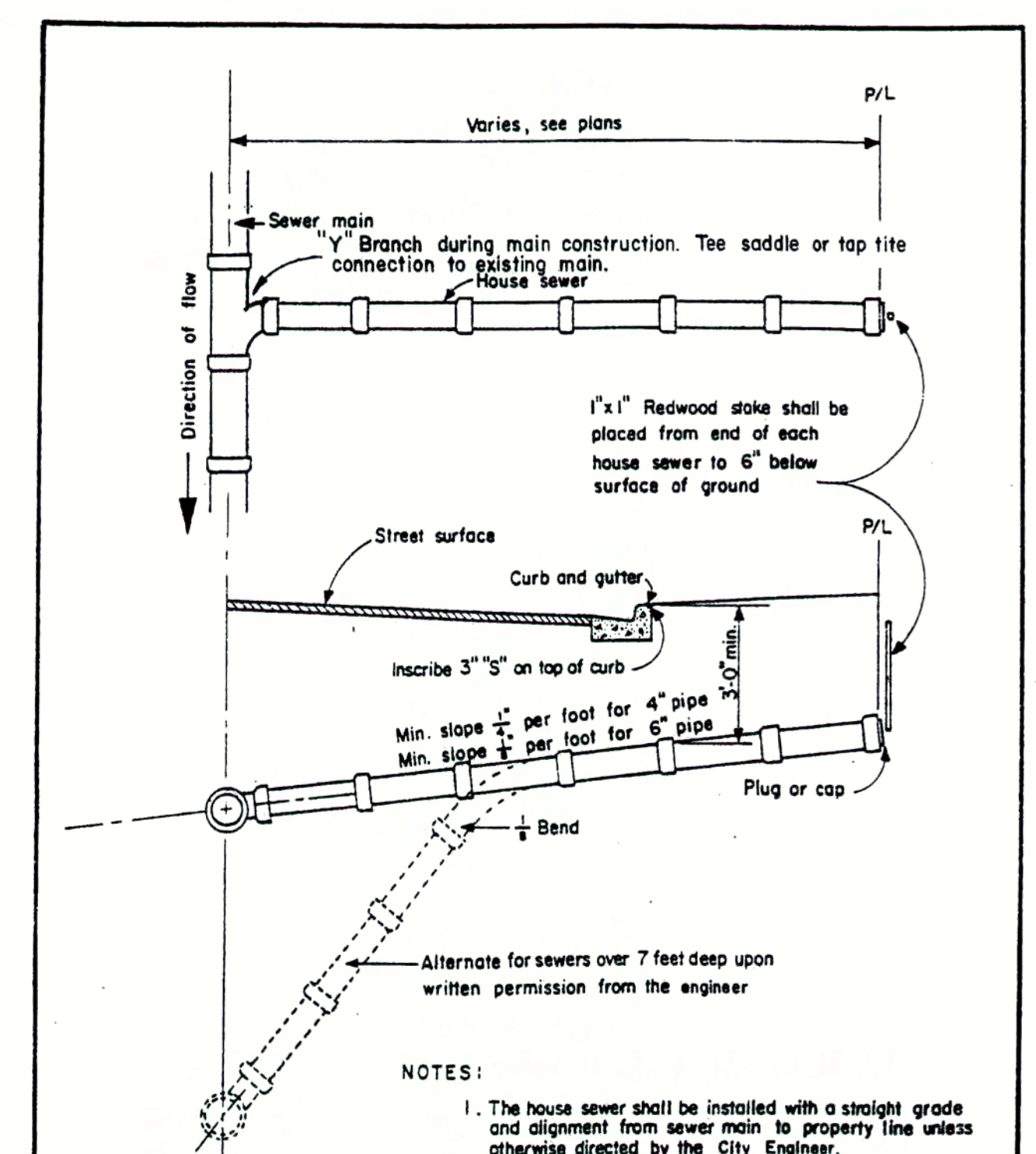
A = Saddle, all bronze
B = Saddle, all bronze, J-976 or H-13490 through H-13494

- 1 Service Saddle, Double Strap Corporation Stop, 3/4", H-10013
- 2 Coupling, 3/4", H-15451
- 3 Copper Tubing, Type K
- 4 Coupling, 3/4", H-15428
- 5 Angle Meter Stop, 3/4", H-14265

Service Size	Dimension A"	Meter Box
3/4"	10"	Christy B-9 or Brooks No. 36 or approved equal
1"	10"	Christy B-16 or Brooks No. 37 or approved equal



		STANDARD COPPER SINGLE WATER SERVICE CONNECTION	DWG. NO. SD-213 FILED 6-15-93 SHT. 1 OF 1
REV.	DATE	BY	
1	4/14/10	JK	



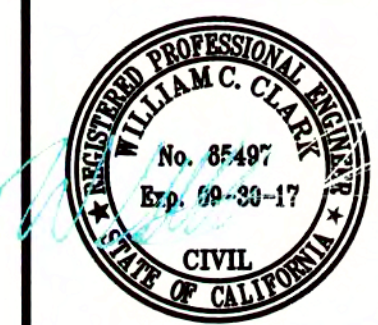
- NOTES:
1. The house sewer shall be installed with a straight grade and alignment from sewer main to property line unless otherwise directed by the City Engineer.
 2. Cleanouts to grade required at house connection and changes in alignment.
 3. Tap may be made to sewer only if lateral is at least one size smaller than the main.

		STANDARD HOUSE SEWER	DWG. NO. SD-312 FILED 6-15-93 SHT. 1 OF 1
REV.	DATE	BY	
1	4/14/10	JK	

REVISIONS	BY

JOB NO: 216026
DATE: 3-21-17
SCALE: AS NOTED
DESIGN BY: WCC
DRAWN BY: DR
SHEET NO:





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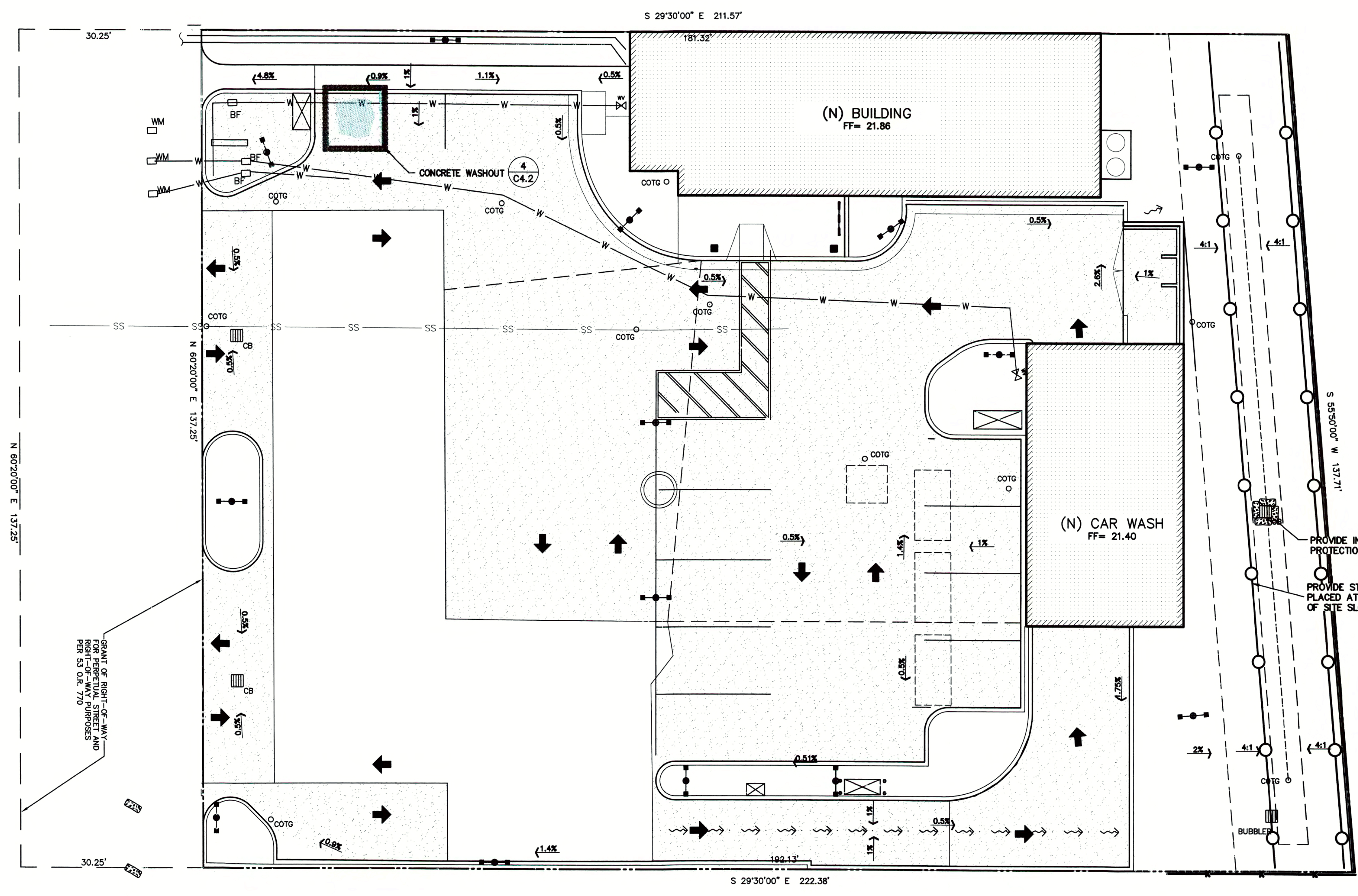


**GAS STATION AND CAR WASH
 EXPANSION AND UPGRADE
 438 WEST TENNYSON RD.
 HAYWARD, CALIFORNIA**
 ALAMEDA COUNTY APN: 465-0001-002

**EROSION CONTROL
 PLAN**

REVISIONS	BY
JOB NO:	216026
DATE:	3-14-18
SCALE:	AS NOTED
DESIGN BY:	WCC
DRAWN BY:	DR
SHEET NO:	

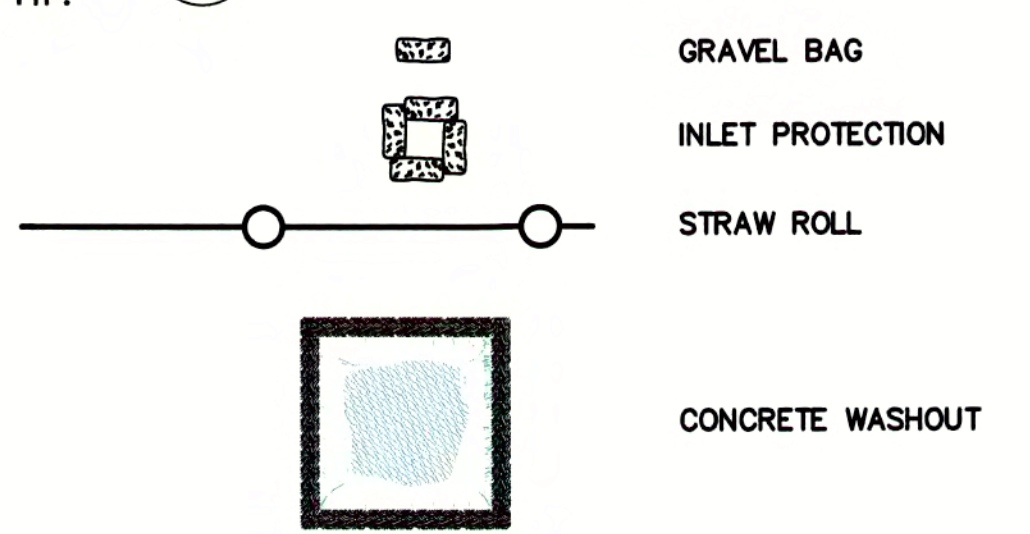
C4.1



PERIODIC MAINTENANCE:

1. MAINTENANCE IS TO BE PERFORMED AS FOLLOWS:
 - A. DAMAGES CAUSED BY SOIL EROSION OR CONSTRUCTION SHALL BE REPAIRED AT THE END OF EACH WORKING DAY.
 - B. SWALES SHALL BE INSPECTED PERIODICALLY AND MAINTAINED AS NEEDED.
 - C. SEDIMENT TRAPS, BERMS, AND SWALES ARE TO BE INSPECTED AFTER EACH STORM AND REPAIRS MADE AS NEEDED.
 - D. SEDIMENT SHALL BE REMOVED AND SEDIMENT TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN SEDIMENT HAS ACCUMULATED TO A DEPTH OF 1' FOOT.
 - E. SEDIMENT REMOVED FROM TRAP SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
 - F. RILLS AND GULLIES MUST BE REPAIRED.
2. GRAVEL BAG INLET PROTECTION SHALL BE CLEANED OUT WHENEVER SEDIMENT DEPTH IS ONE HALF THE HEIGHT OF ONE GRAVEL BAG.
3. STRAW ROLLS SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHED HALF THE HEIGHT OF THE ROLL.
4. SILT FENCE SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHES ONE FOOT IN HEIGHT.
5. CONSTRUCTION ENTRANCE SHALL BE REGRAVELED AS NECESSARY FOLLOWING SILT/SOIL BUILDUP.
6. ANY OTHER EROSION CONTROL MEASURES SHOULD BE CHECKED AT REGULAR INTERVALS TO ASSURE PROPER FUNCTION

EROSION CONTROL LEGEND



NOTE:
 SEAL ALL OTHER INLETS NOT INTENDED TO ACCEPT STORM WATER AND DIRECT FLOWS TEMPORARILY TO FUNCTIONAL SEDIMENTATION BASIN INLETS. -TYP

PROVIDE TEMPORARY GRAVEL BAGS OR STRAW BALES AT FLOW LINE AS SHOWN (2) C4.2

PROVIDE INLET PROTECTION -TYP. (1) C4.2

PROVIDE STRAW ROLLS PLACED AT THE PERIMETER OF SITE SLOPES -TYP. (3) C4.2

GRANT OF RIGHT-OF-WAY FOR PERPETUAL STREET AND RIGHT-OF-WAY PURPOSES PER S.D. 634.770

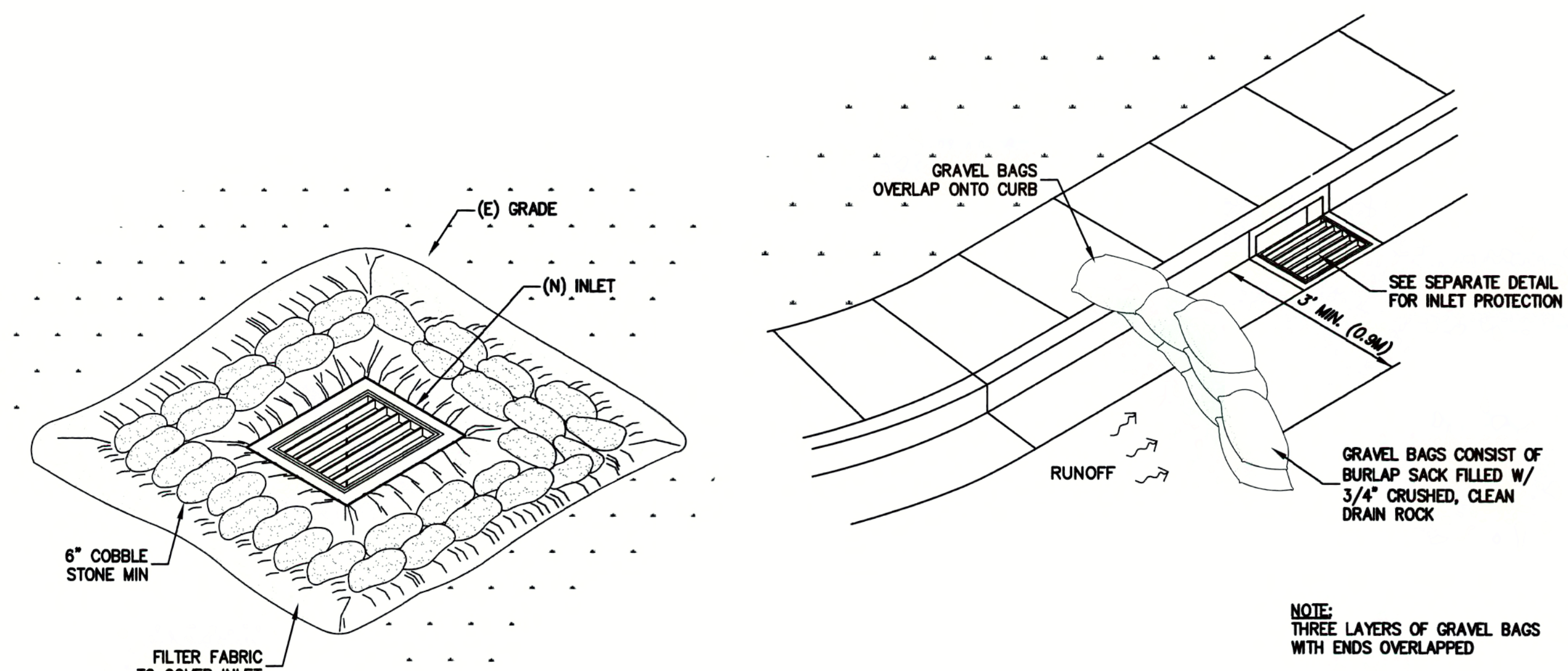


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GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD.
HAYWARD, CALIFORNIA
ALAMEDA COUNTY APN: 465-0001-002

EROSION CONTROL
DETAILS



1 INLET PROTECTION
C4.2 NTS

2 GRAVEL BAG DIKE
C4.2 NTS

PURPOSE:

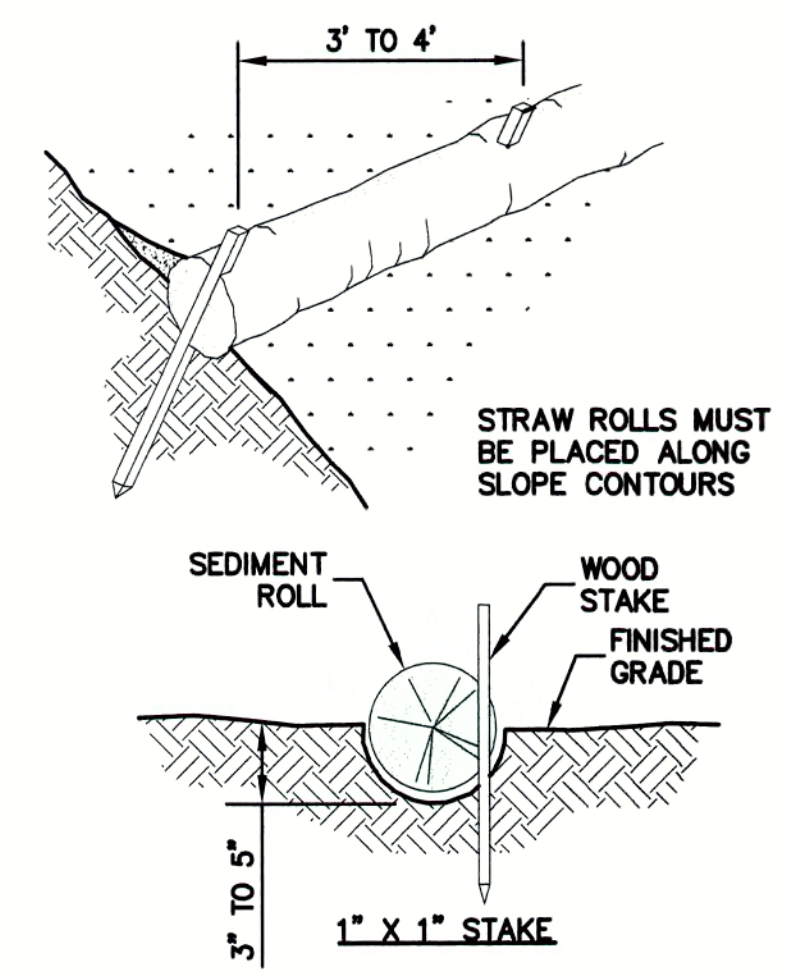
THE PURPOSE OF THIS PLAN IS TO STABILIZE THE SITE TO PREVENT EROSION OF GRADED AREAS AND TO PREVENT SEDIMENTATION FROM LEAVING THE CONSTRUCTION AREA AND AFFECTING NEIGHBORING SITES, NATURAL AREAS, PUBLIC FACILITIES OR ANY OTHER AREA THAT MIGHT BE AFFECTED BY SEDIMENTATION. ALL MEASURES SHOWN ON THIS PLAN SHOULD BE CONSIDERED THE MINIMUM REQUIREMENTS NECESSARY. SHOULD FIELD CONDITIONS DICTATE ADDITIONAL MEASURES, SUCH MEASURES SHALL BE PER CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, AND THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION. LEA & BRAZE ENGINEERING SHOULD BE NOTIFIED IMMEDIATELY SHOULD CONDITIONS CHANGE.

EROSION CONTROL MEASURES:

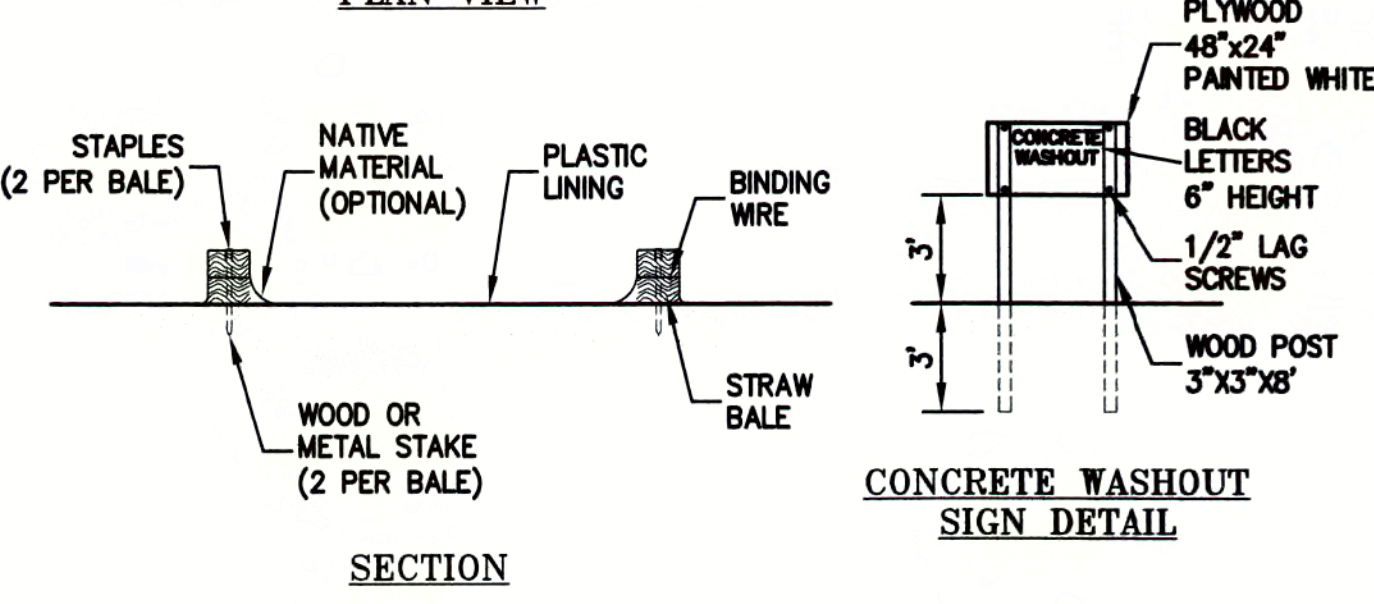
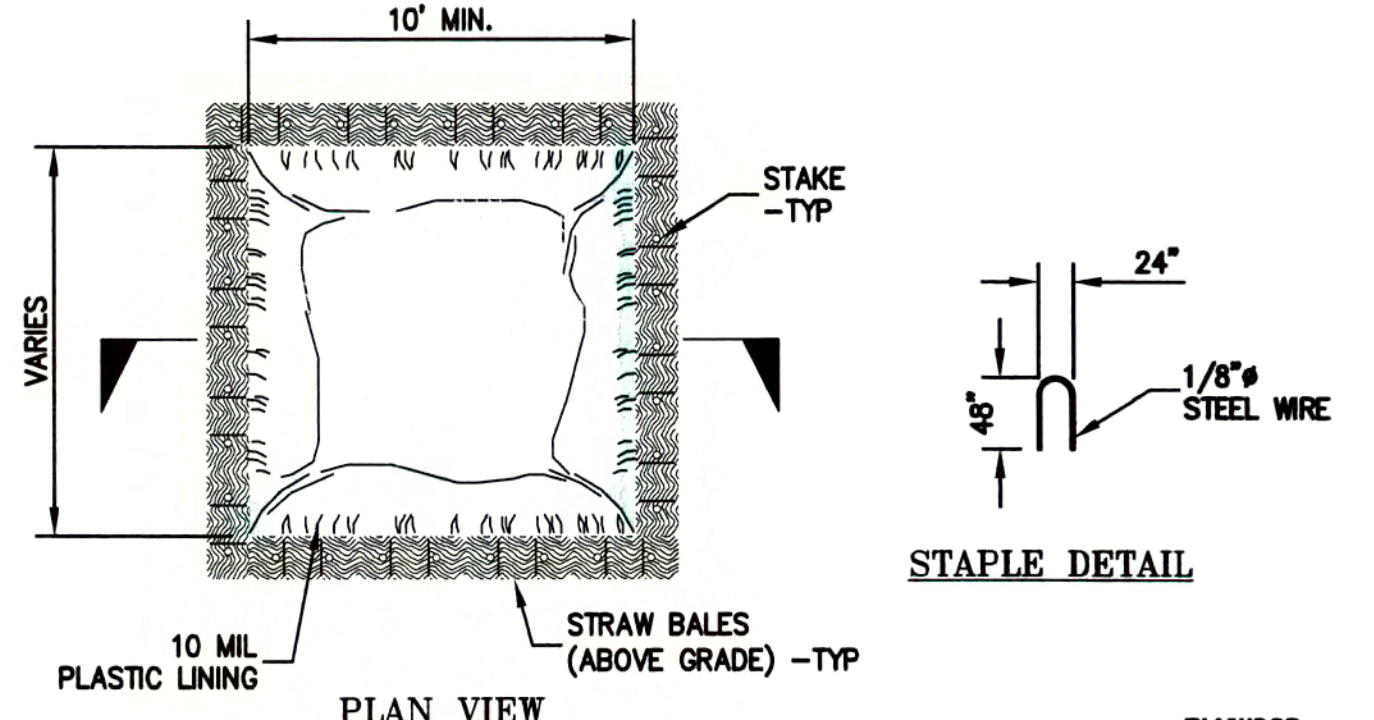
1. THE FACILITIES SHOWN ON THIS PLAN ARE DESIGNED TO CONTROL EROSION AND SEDIMENT DURING THE RAINY SEASON, OCTOBER 15TH TO APRIL 15. EROSION CONTROL FACILITIES SHALL BE IN PLACE PRIOR TO OCTOBER 15TH OF ANY YEAR. GRADING OPERATIONS DURING THE RAINY SEASON WHICH LEAVE DENUDED SLOPES SHALL BE PROTECTED WITH EROSION CONTROL MEASURES IMMEDIATELY FOLLOWING GRADING ON THE SLOPE.
2. SITE CONDITIONS AT TIME OF PLACEMENT OF EROSION CONTROL MEASURES WILL VARY. APPROPRIATE ACTION INCLUDING TEMPORARY SWALES, INLETS, HYDROSEEDING, STRAW BALES, ROCK SACKS, ETC. SHALL BE TAKEN TO PREVENT EROSION AND SEDIMENTATION FROM LEAVING SITE. EROSION CONTROL MEASURES SHALL BE ADJUSTED AS THE CONDITIONS CHANGE AND THE NEED OF CONSTRUCTION SHIFT.
3. CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. ALL CONSTRUCTION TRAFFIC ENTERING ONTO THE PAVED ROADS MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCES. CONTRACTOR SHALL MAINTAIN STABILIZED ENTRANCE AT EACH VEHICLE ACCESS POINT TO EXISTING PAVED STREETS. ANY MUD OR DEBRIS TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED DAILY AND AS REQUIRED BY THE GOVERNING AGENCY.
4. ALL EXPOSED SLOPES THAT ARE NOT VEGETATED SHALL BE HYDROSEED. IF HYDROSEEDING IS NOT USED OR IS NOT EFFECTIVE BY OCTOBER 15, THEN OTHER IMMEDIATE METHODS SHALL BE IMPLEMENTED, SUCH AS EROSION CONTROL BLANKETS, OR A THREE-STEP APPLICATION OF 1) SEED, MULCH, FERTILIZER 2) BLOWN STRAW 3) TACKIFIER AND MULCH. HYDROSEEDING SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF SECTION 20" EROSION CONTROL AND HIGHWAY PLANTING" OF THE STANDARD SPECIFICATION OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION, AS LAST REVISED. REFER TO THE EROSION CONTROL SECTION OF THE GRADING SPECIFICATIONS THAT ARE A PART OF THIS PLAN SET FOR FURTHER INFORMATION.
5. INLET PROTECTION SHALL BE INSTALLED AT OPEN INLETS TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAIN SYSTEM. INLETS NOT USED IN CONJUNCTION WITH EROSION CONTROL ARE TO BE BLOCKED TO PREVENT ENTRY OF SEDIMENT. MINIMUM INLET PROTECTION SHALL CONSIST OF A ROCK SACKS OR AS SHOWN ON THIS PLAN
6. THIS EROSION AND SEDIMENT CONTROL PLAN MAY NOT COVER ALL THE SITUATIONS THAT MAY ARISE DURING CONSTRUCTION DUE TO UNANTICIPATED FIELD CONDITIONS. VARIATIONS AND ADDITIONS MAY BE MADE TO THIS PLAN IN THE FIELD. A REPRESENTATIVE OF LEA & BRAZE ENGINEERING SHALL PERFORM A FIELD REVIEW AND MAKE RECOMMENDATIONS AS NEEDED. CONTRACTOR IS RESPONSIBLE TO NOTIFY LEA & BRAZE ENGINEERING AND THE GOVERNING AGENCY OF ANY CHANGES.
7. THE EROSION CONTROL MEASURES SHALL CONFORM TO THE LOCAL JURISDICTION'S STANDARDS AND THE APPROVAL OF THE LOCAL JURISDICTION'S ENGINEERING DEPARTMENT.
8. STRAW ROLLS SHALL BE PLACED AT THE TOE OF SLOPES AND ALONG THE DOWN SLOPE PERIMETER OF THE PROJECT. THEY SHALL BE PLACED AT 25 FOOT INTERVALS ON GRADED SLOPES. PLACEMENT SHALL RUN WITH THE CONTOURS AND ROLLS SHALL BE TIGHTLY END BUTTED. CONTRACTOR SHALL REFER TO MANUFACTURERS SPECIFICATIONS FOR PLACEMENT AND INSTALLATION INSTRUCTIONS.

EROSION CONTROL NOTES:

1. IT SHALL BE THE OWNER'S/CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CONTROL OF THE ENTIRE CONSTRUCTION OPERATION AND TO KEEP THE ENTIRE SITE IN COMPLIANCE WITH THIS EROSION CONTROL PLAN.
2. THE INTENTION OF THIS PLAN IS FOR INTERIM EROSION AND SEDIMENT CONTROL ONLY. ALL EROSION CONTROL MEASURES SHALL CONFORM TO CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION, AND THE LOCAL GOVERNING AGENCY FOR THIS PROJECT.
3. OWNER/CONTRACTOR SHALL BE RESPONSIBLE FOR MONITORING EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO, DURING, AND AFTER STORM EVENTS. PERSON IN CHARGE OF MAINTAINING EROSION CONTROL MEASURES SHOULD WATCH LOCAL WEATHER REPORTS AND ACT APPROPRIATELY TO MAKE SURE ALL NECESSARY MEASURES ARE IN PLACE.
4. SANITARY FACILITIES SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
5. DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT-LADEN RUNOFF TO ANY STORM DRAINAGE SYSTEM, INCLUDING EXISTING DRAINAGE SWALES AND WATERCOURSES.
6. CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION WILL BE MINIMIZED. COMPLIANCE WITH FEDERAL, STATE AND LOCAL LAWS CONCERNING POLLUTION SHALL BE MAINTAINED AT ALL TIMES.
7. CONTRACTOR SHALL PROVIDE DUST CONTROL AS REQUIRED BY THE APPROPRIATE FEDERAL, STATE AND LOCAL AGENCY REQUIREMENTS.
8. ALL MATERIALS NECESSARY FOR THE APPROVED EROSION CONTROL MEASURES SHALL BE IN PLACE BY OCTOBER 15TH.
9. EROSION CONTROL SYSTEMS SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON, OR FROM OCTOBER 15TH THROUGH APRIL 15TH, WHICHEVER IS LONGER.
10. IN THE EVENT OF RAIN, ALL GRADING WORK IS TO CEASE IMMEDIATELY AND THE SITE IS TO BE SEALED IN ACCORDANCE WITH THE APPROVAL EROSION CONTROL MEASURES AND APPROVED EROSION CONTROL PLAN.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING AND REPAIRING EROSION CONTROL SYSTEMS AFTER EACH STORM.
12. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY LOCAL JURISDICTION'S ENGINEERING DEPARTMENT OR BUILDING OFFICIALS.
13. MEASURES SHALL BE TAKEN TO COLLECT OR CLEAN ANY ACCUMULATION OR DEPOSIT OF DIRT, MUD, SAND, ROCKS, GRAVEL OR DEBRIS ON THE SURFACE OF ANY STREET, ALLEY OR PUBLIC PLACE OR IN ANY PUBLIC STORM DRAIN SYSTEMS. THE REMOVAL OF AFORESAID SHALL BE DONE BY STREET SWEEPING OR HAND SWEEPING. WATER SHALL NOT BE USED TO WASH SEDIMENTS INTO PUBLIC OR PRIVATE DRAINAGE FACILITIES.
14. EROSION CONTROL MEASURES SHALL BE ON-SITE FROM SEPTEMBER 15TH THRU APRIL 15TH.
15. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON OR FROM OCTOBER 15 THRU APRIL 15, WHICHEVER IS GREATER.



3 STRAW ROLLS
C4.2 NTS



4 CONCRETE WASHOUT
C4.2 NTS

NOTES:
ACTUAL LAYOUT DETERMINED IN FIELD.
THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 10' OF THE TEMPORARY CONCRETE WASHOUT FACILITY.

REFERENCES:

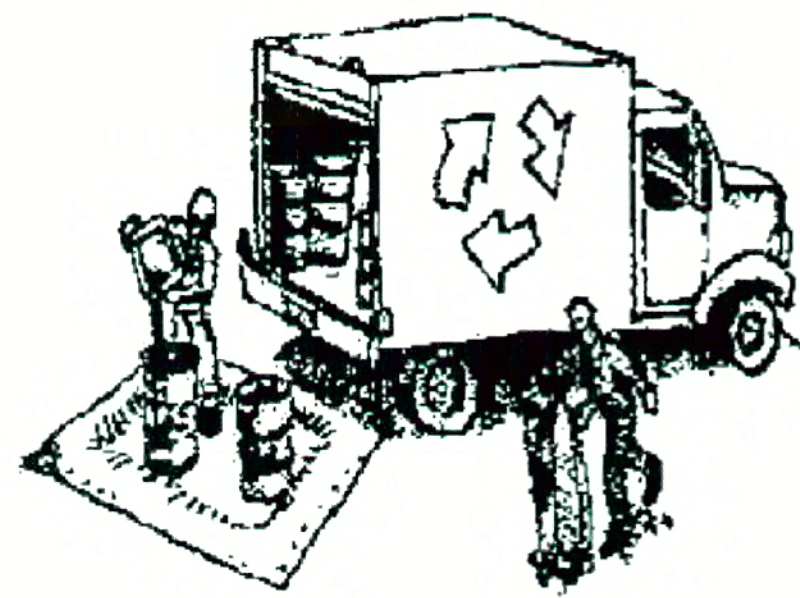
1. CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL.
2. CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION

REVISIONS	BY
JOB NO:	216026
DATE:	XX-2XX+XX
SCALE:	AS NOTED
DESIGN BY:	WCC
DRAWN BY:	DR
SHEET NO:	

Construction Best Management Practices (BMPs)

Construction projects are required to implement the stormwater best management practices (BMP) on this page, as they apply to your project, all year long.

Materials & Waste Management



Non-Hazardous Materials

- Berm and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or if not actively being used within 14 days.
- Use (but don't overuse) reclaimed water for dust control.

Hazardous Materials

- Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
- Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
- Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals outdoors when rain is forecast within 24 hours.
- Arrange for appropriate disposal of all hazardous wastes.

Waste Management

- Cover waste disposal containers securely with tarps at the end of every work day and during wet weather.
- Check waste disposal containers frequently for leaks and to make sure they are not overfilled. Never hose down a dumpster on the construction site.
- Clean or replace portable toilets, and inspect them frequently for leaks and spills.
- Dispose of all wastes and debris properly. Recycle materials and wastes that can be recycled (such as asphalt, concrete, aggregate base materials, wood, gyp board, pipe, etc.)
- Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.

Construction Entrances and Perimeter

- Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharges from site and tracking off site.
- Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking. Never hose down streets to clean up tracking.

Equipment Management & Spill Control



Maintenance and Parking

- Designate an area, fitted with appropriate BMPs, for vehicle and equipment parking and storage.
- Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
- If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
- If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
- Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, steam cleaning equipment, etc.

Spill Prevention and Control

- Keep spill cleanup materials (rags, absorbents, etc.) available at the construction site at all times.
- Inspect vehicles and equipment frequently for and repair leaks promptly. Use drip pans to catch leaks until repairs are made.
- Clean up spills or leaks immediately and dispose of cleanup materials properly.
- Do not hose down surfaces where fluids have spilled. Use dry cleanup methods (absorbent materials, cat litter, and/or rags).
- Sweep up spilled dry materials immediately. Do not try to wash them away with water, or bury them.
- Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
- Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill: 1) Dial 911 or your local emergency response number, 2) Call the Governor's Office of Emergency Services Warning Center, (800) 852-7550 (24 hours).

Earthwork & Contaminated Soils



Erosion Control

- Schedule grading and excavation work for dry weather only.
- Stabilize all denuded areas, install and maintain temporary erosion controls (such as erosion control fabric or bonded fiber matrix) until vegetation is established.
- Seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.

Sediment Control

- Protect storm drain inlets, gutters, ditches, and drainage courses with appropriate BMPs, such as gravel bags, fiber rolls, berms, etc.
- Prevent sediment from migrating offsite by installing and maintaining sediment controls, such as fiber rolls, silt fences, or sediment basins.
- Keep excavated soil on the site where it will not collect into the street.
- Transfer excavated materials to dump trucks on the site, not in the street.
- Contaminated Soils
- If any of the following conditions are observed, test for contamination and contact the Regional Water Quality Control Board:
 - Unusual soil conditions, discoloration, or odor.
 - Abandoned underground tanks.
 - Abandoned wells
 - Buried barrels, debris, or trash.

Paving/Asphalt Work

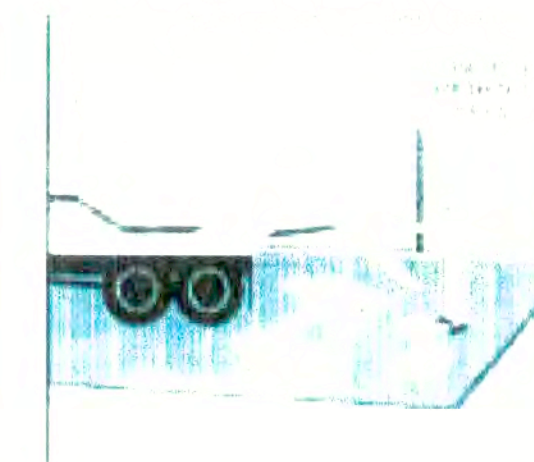


- Avoid paving and seal coating in wet weather, or when rain is forecast before fresh pavement will have time to cure.
- Cover storm drain inlets and manholes when applying seal coat, tack coat, slurry seal, fog seal, etc.
- Collect and recycle or appropriately dispose of excess abrasive gravel or sand. Do NOT sweep or wash it into gutters.
- Do not use water to wash down fresh asphalt concrete pavement.

Sawcutting & Asphalt/Concrete Removal

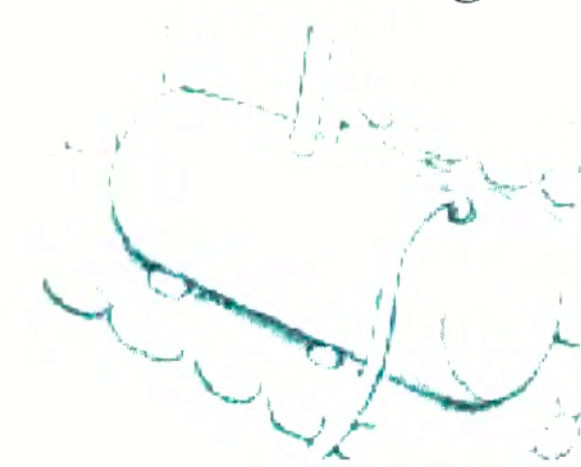
- Completely cover or barricade storm drain inlets when saw cutting. Use filter fabric, catch basin inlet filters, or gravel bags to keep slurry out of the storm drain system.
- Shovel, absorb, or vacuum saw-cut slurry and dispose of all waste as soon as you are finished in one location or at the end of each work day (whichever is sooner!).
- If sawcut slurry enters a catch basin, clean it up immediately.

Concrete, Grout & Mortar Application



- Store concrete, grout and mortar under cover, on pallets and away from drainage areas. These materials must never reach a storm drain.
- Wash out concrete equipment/trucks offsite or in a contained area, so there is no discharge into the underlying soil or onto surrounding areas. Let concrete harden and dispose of as garbage.
- Collect the wash water from washing exposed aggregate concrete and remove it for appropriate disposal offsite.

Dewatering



- Effectively manage all run-on, all runoff within the site, and all runoff that discharges from the site. Divert run-on water from offsite away from all disturbed areas or otherwise ensure compliance.
- When dewatering, notify and obtain approval from the local municipality before discharging water to a street gutter or storm drain. Filtration or diversion through a basin, tank, or sediment trap may be required.
- In areas of known contamination, testing is required prior to reuse or discharge of groundwater. Consult with the Engineer to determine whether testing is required and how to interpret results. Contaminated groundwater must be treated or hauled off-site for proper disposal.

Painting & Paint Removal



Painting cleanup

- Never clean brushes or rinse paint containers into a street, gutter, storm drain, or surface waters.
- For water-based paints, paint out brushes to the extent possible. Rinse to the sanitary sewer once you have gained permission from the local wastewater treatment authority. Never pour paint down a drain.
- For oil-based paints, paint out brushes to the extent possible and clean with thinner or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of residue and unusable thinner/solvents as hazardous waste.

Paint removal

- Chemical paint stripping residue and chips and dust from marine paints or paints containing lead or tributyltin must be disposed of as hazardous waste.
- Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic drop cloths and disposed of as trash.

Landscape Materials



- Contain stockpiled landscaping materials by storing them under tarps when they are not actively being used.
- Stack erodible landscape material on pallets. Cover or store these materials when they are not actively being used or applied.
- Discontinue application of any erodible landscape material within 2 days before a forecast rain event or during wet weather.

Storm drain polluters may be liable for fines of up to \$10,000 per day!



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GAS STATION AND CAR WASH
EXPANSION AND UPGRADE
438 WEST TENNYSON RD.
HAYWARD, CALIFORNIA
ALAMEDA COUNTY APN: 465-0001-002

CONSTRUCTION
BEST MANAGEMENT
PRACTICES (SWPPP)

REVISIONS	BY
JOB NO:	216026
DATE:	3-21-17
SCALE:	AS NOTED
DESIGN BY:	WCC
DRAWN BY:	DR
SHEET NO:	

C4.3

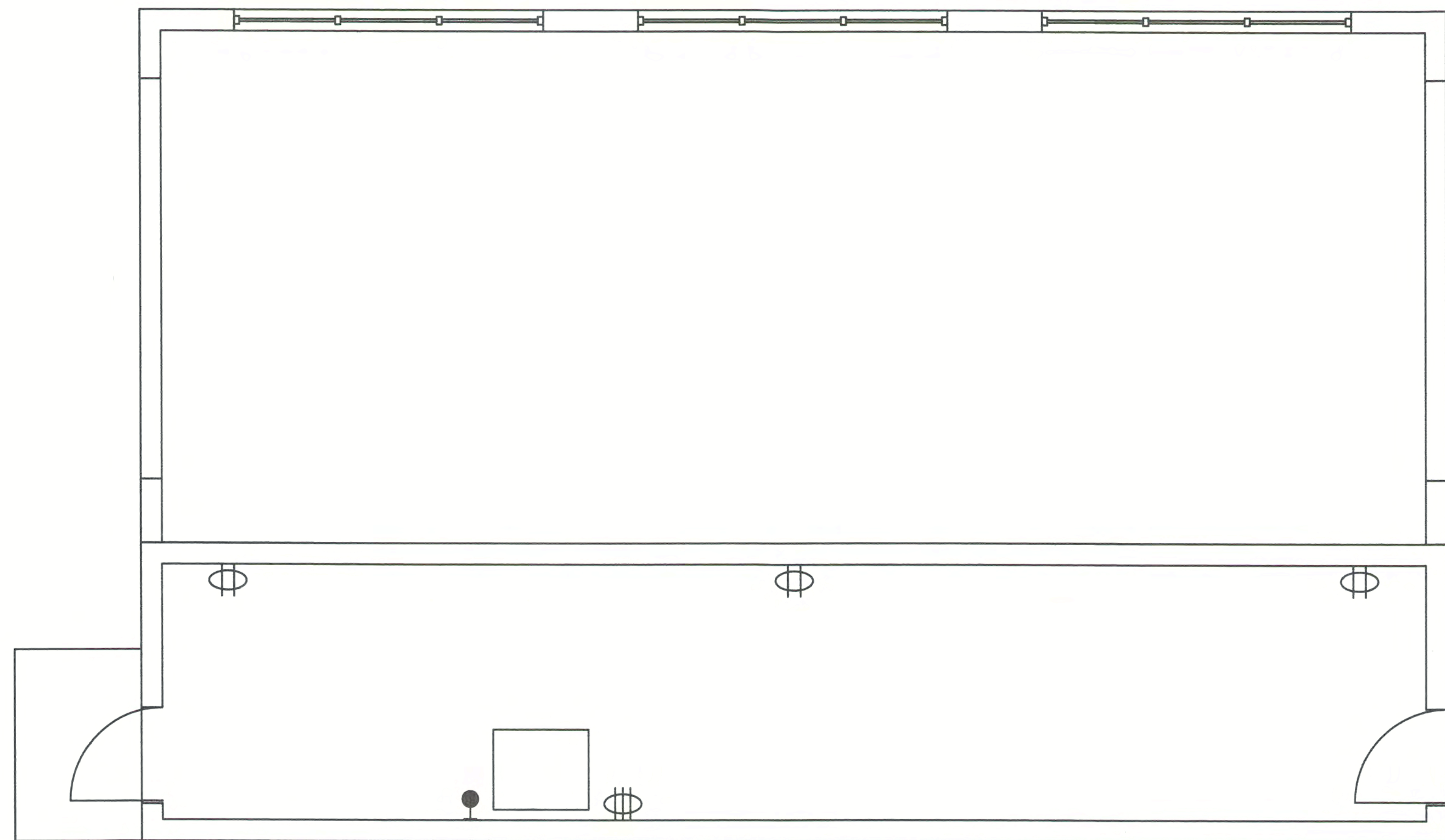
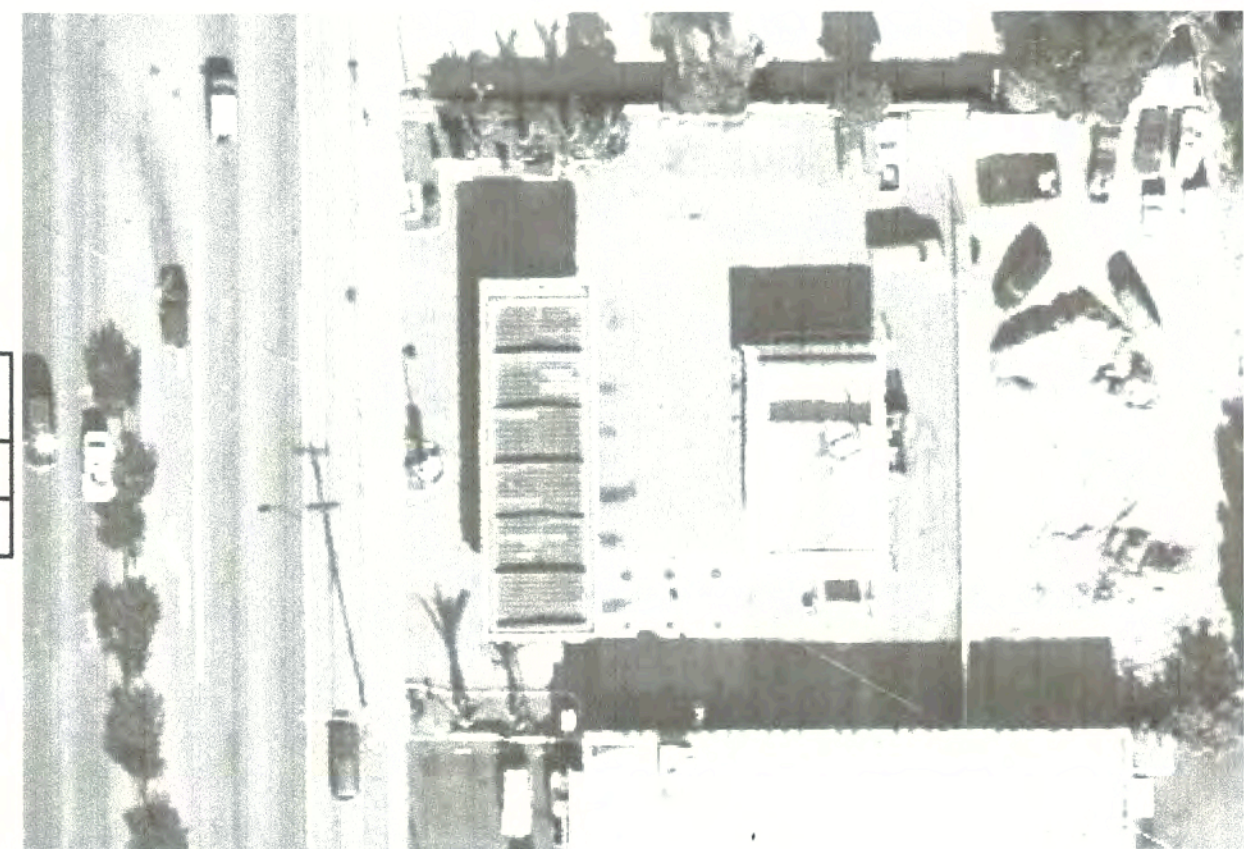
SONNY'S CARWASH DESIGN SERVICES

PROJECT: "PETER XU-HAYWARD"

Prefix	Project N°	Sheet	Sheet Title
CWP	-15442-C11-	0	(INDEX)
CWP	-15442-C11-	00	(RENDERED TUNNEL LAYOUT)
CWP	-15442-C11-	01	(SITE LAYOUT)
CWP	-15442-C11-	02	(TRENCH PIT LAYOUT; <u>GC</u>)
CWP	-15442-C11-	L.1	(EQUIPMENT LAYOUT)
CWP	-15442-C11-	L.2	(EQ. TUNNEL DESCRIPTION)
CWP	-15442-C11-	L.3	(TUNNEL EQUIPMENT INSTALLATION)
CWP	-15442-C11-	B.1	(EQ. SUPPORT DESCRIPTION)
CWP	-15442-C11-	M.1	(EQUIPMENT LIST-1)
CWP	-15442-C11-	M.2	(EQUIPMENT LIST-2)
CWP	-15442-C11-	P.1	(PLUMBING; <u>GC</u>)
CWP	-15442-C11-	P.3	(COLD WATER; <u>GC</u>)
CWP	-15442-C11-	P.7	(RECLAIM WATER.; <u>GC</u>)
CWP	-15442-C11-	P.8	(RECLAIM TANK SYSTEM; <u>GC</u>)
CWP	-15442-C11-	P.8A	(TRASH PIT; <u>GC</u>)
CWP	-15442-C11-	P.9	(RECLAIM SYSTEM; <u>GC</u>)
CWP	-15442-C11-	P.10	(PNEUMATIC; <u>GC</u>)
CWP	-15442-C11-	P.12	(CHASE WAY; <u>GC</u>)
CWP	-15442-C11-	P.13	(CORED HOLES; <u>GC</u>)
CWP	-15442-C11-	P.14	(LEGEND)
CWP	-15442-C11-	P.15	(DIVISION OF WORK)
CWP	-15442-C11-	V.2	(VACUUM LAYOUT; <u>GC</u>)
CWP	-15442-C11-	V.3	(VACUUM LAYOUT; <u>GC</u>)
CWP	-15442-C11-	V.4	(VACUUM LAYOUT; <u>GC</u>)
CWP	-15442-C11-	V.5	(VACUUM LAYOUT; <u>GC</u>)
CWP	-15442-C11-	E.1	(ELECTRICAL; <u>GC</u>)
CWP	-15442-C11-	E.2	(CONTROL VOLTAGE; <u>GC</u>)
CWP	-15442-C11-	E.3	(MOTOR RUNS; <u>GC</u>)
CWP	-15442-C11-	E.4	(ONE LINE ELECTRICAL)
CWP	-15442-C11-	E.5	(ELECTRICAL PANEL; <u>GC</u>)
CWP	-15442-C11-	E.6	(DIVISION OF WORK/LEGEND; ELECTRICAL)
CWP	-15442-C11-	E.7	(TIMING DISTANCES)
CWC	-15442-C11-	C.1	(SECTION THRU CORRELATOR; <u>GC</u>)
CWT	-15442-C11-	T.1	(CONV. TRENCH PLAN VIEW)
CWT	-15442-C11-	S.1	(SECTION THRU CONV. & PIT; <u>GC</u>)
CWT	-15442-C11-	S.2	(SECTION THRU TUNNEL; <u>GC</u>)
CWT	-15442-C11-	S.3	(SECTION THRU CONV.; <u>GC</u>)

DATE	SYM.	REVISION RECORD	TIME	DR.	AUTH.
07/14/17	A	NEW EQUIPMENT; REMOVED SITE	12 HRS	AC	LV
SUPERSEDES DWG. OF: 04/14/16					

NEW ITEMS	DELETED ITEMS
5-2	5-4
10-9	10-13
39-C	39-B



DATE:	11.11.15
REVISION DATE:	07.14.17
CREATED BY:	LV
REVISED BY:	ADC
CHECKED BY:	Luis V.

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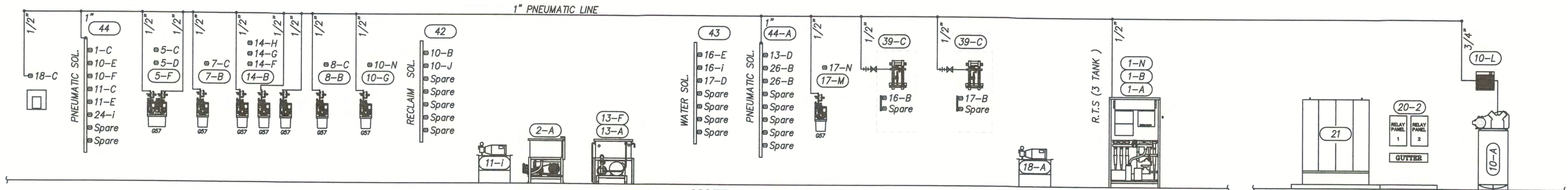
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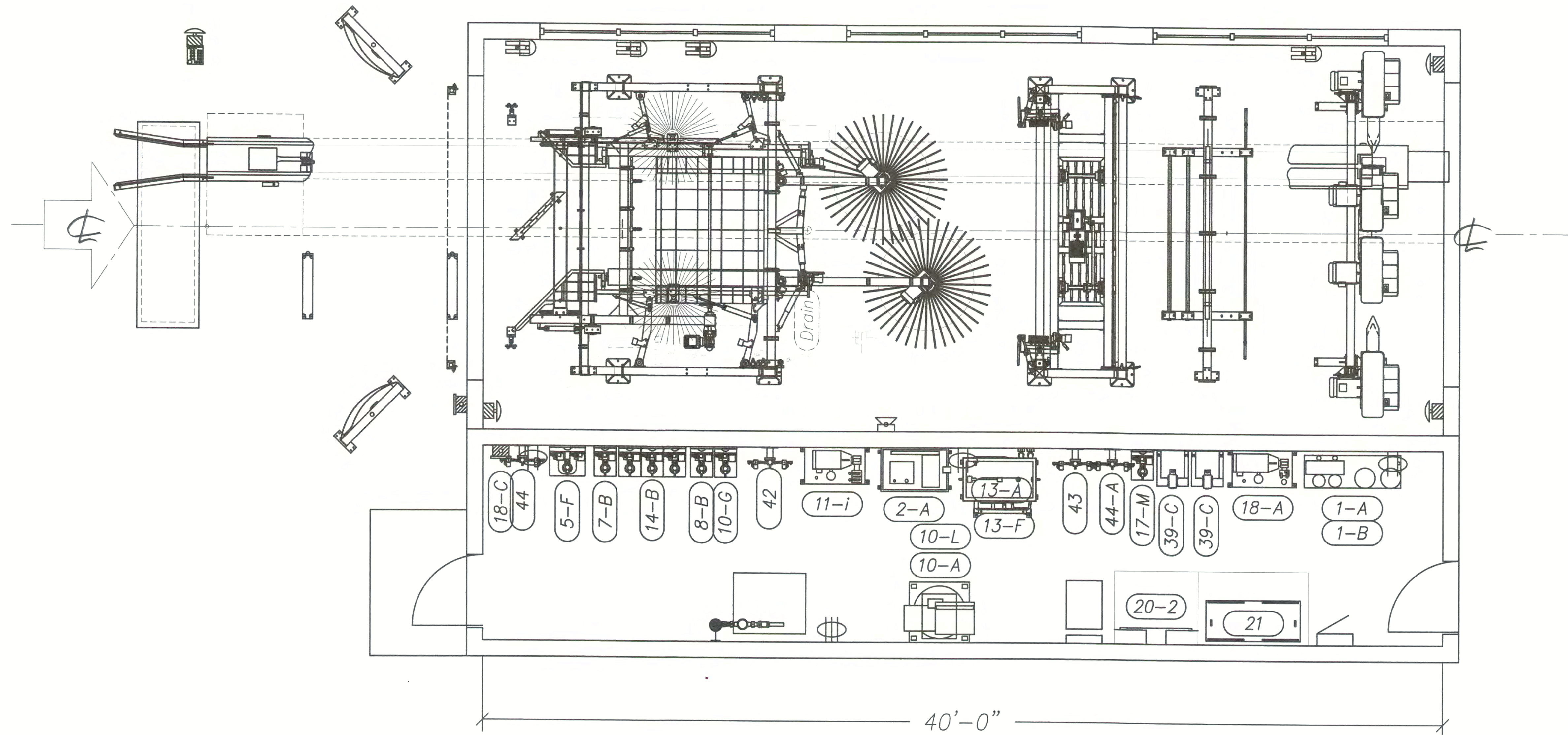
Sheet	Rev.
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PNEUMATIC

**TUNNEL WALL
EQUIPMENT ROOM**

NOTE:
1-SEE LEGEND SHEET
2-CONNECTIONS SHOWN IN THE ELEVATION VIEW ARE SCHEMATIC ONLY. REFER TO PLAN VIEWS FOR PLACEMENT.



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SCALE: 3/16"=1'

LOCATION: CA

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P.10 -A-

PNEUMATIC; GC

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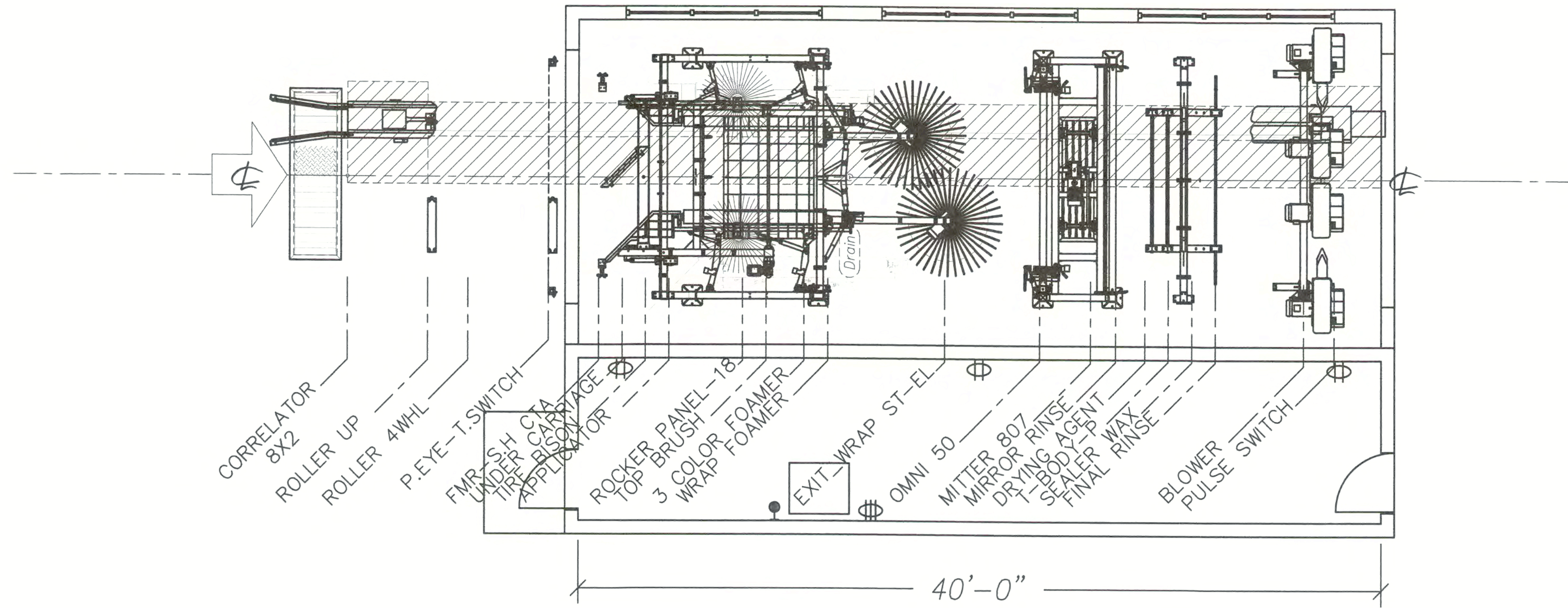
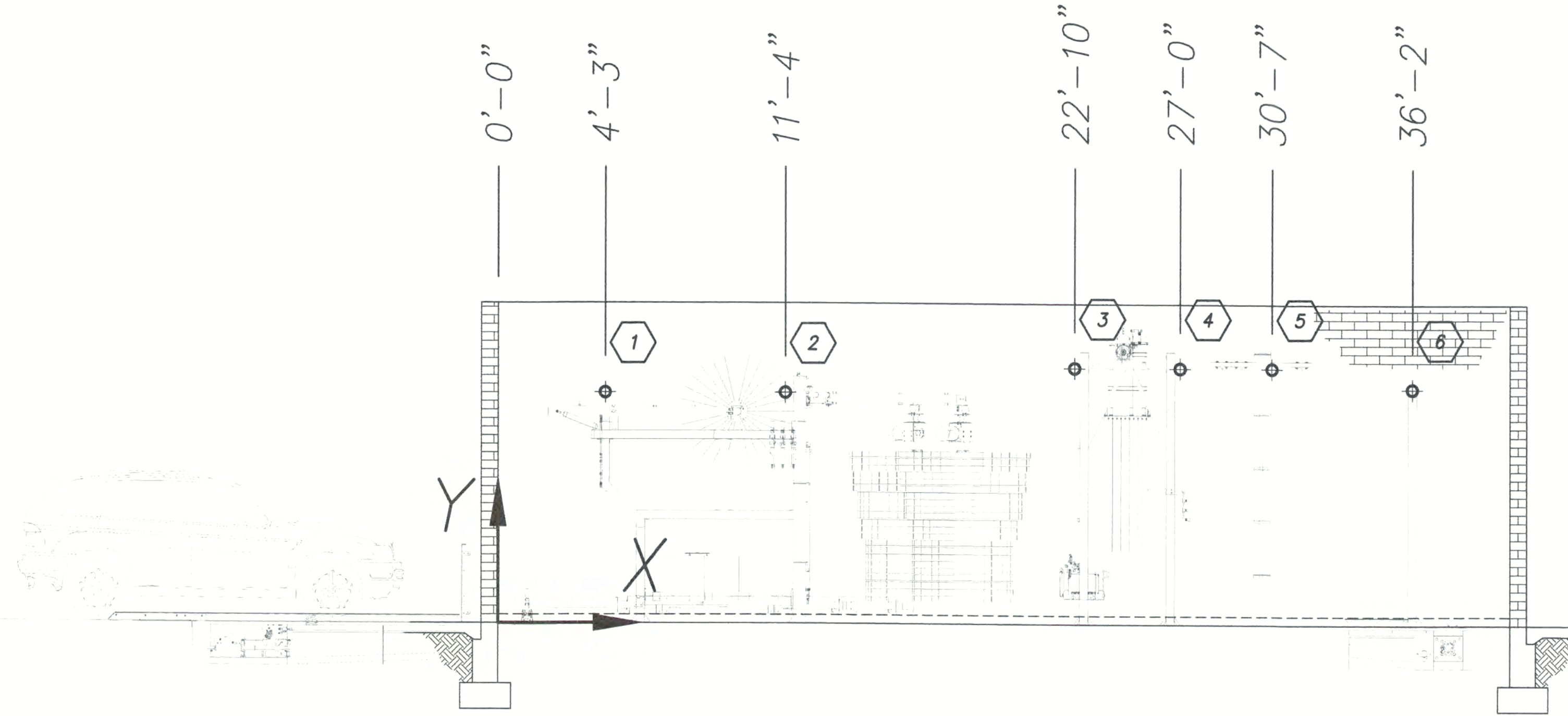
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CHECKED BY: Luis V.

ELEVATION

Ø 4-1/2" OPENING IN WALL
 WALL Ø 4" PVC PIPE &
 FINISH CAULKING BY GC.

REF.	COORD. C-LINE	Y
1	TOP BRUSH COMBO	108"
2	TOP BRUSH COMBO	108"
3	MITTER SFM807	120"
4	MITTER SFM807	120"
5	RAINMA ARCH 10'	120"
6	BLOWER ARCH/S	108"



NOTES:

PRELIMINARY LAYOUT. FINAL LAYOUT SUBJECT TO FINAL EQUIPMENT LIST APPROVAL DO NOT CORE UNTIL APPROVED BY INSTALLER.

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 APPROVED BY. DATE

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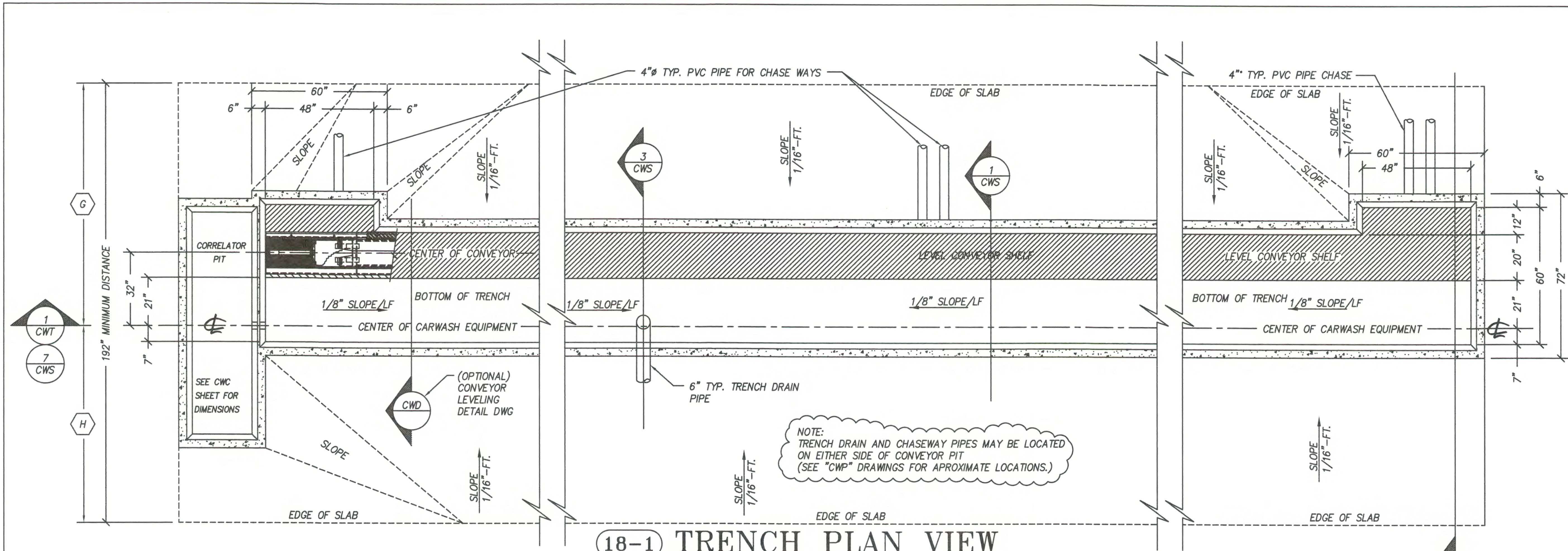
CWP-15442-C11-XU
 SCALE: 1/8"=1' LOCATION: CA

Sheet Rev.
 P.13 -A-

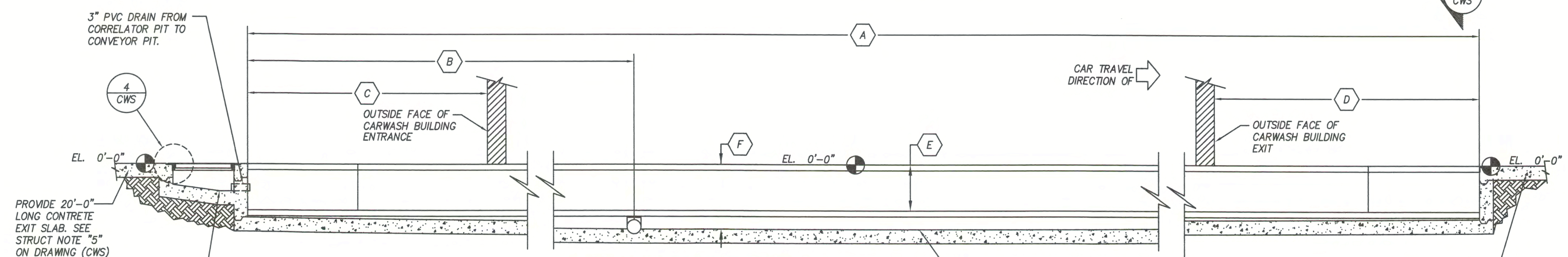
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CORED HOLES PLAN; GC
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18-1 TRENCH PLAN VIEW



DIMENSION LEGEND		
TAG	DESCRIPTION	DIMENSION
A	TRENCH CONVEYOR PIT LENGTH	51'6"
B	CENTER LINE DRAIN EDGE OF CONVEYOR PIT	25'-0"
C	TRENCH PIT OUTSIDE BUILDING (ENTRANCE)	10'-10"
D	TRENCH PIT OUTSIDE BUILDING (EXIT)	N/A
E	CONVEYOR SHELF DEPTH FRONT-W-PULL=21-3/4" REAR-W-PUSH=19-3/4"	21-3/4"
F	MAX DEPTH OF TRENCH	28"
G	CENTERLINE OF EQUIPMENT TO DRIVERS SIDE WALL	7'-9"
H	CENTERLINE OF EQUIPMENT TO PASSENGER SIDE WALL	8'-3"

NOTE:
 - THE FINISHED FLOOR SHOULD BE LEVEL FROM ENTRANCE TO EXIT, HOWEVER, A 1" RISE EVERY 50 FT IS PERMISSIBLE WITH A MAXIMUM 2" RISE OVER THE ENTIRE LENGTH.
 - SEE TYPICAL TRENCH SECTION FOR REQUIRED CONSTRUCTION TOLERANCES FOR TRENCH CONSTRUCTION.

7 CWS NOTE: CORRELATOR PIT

PROVIDE 20'-0" LONG CONCRETE EXIT SLAB. SEE STRUCT NOTE "5" ON DRAWING (CWS)

PROVIDE 20'-0" LONG CONCRETE EXIT SLAB. SEE STRUCT NOTE "5" ON DRAWING (CWS)

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 APPROVED BY: _____ DATE: _____

TRENCH ELEVATION

1 CWT
18-1

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 SCALE: 1/4"=1'-0"

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T.1	-A-

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CONVEYOR TRENCH PLAN VIEW
 CONVEYOR PIT DESIGN - NO DAM
 GENERIC (12-11)

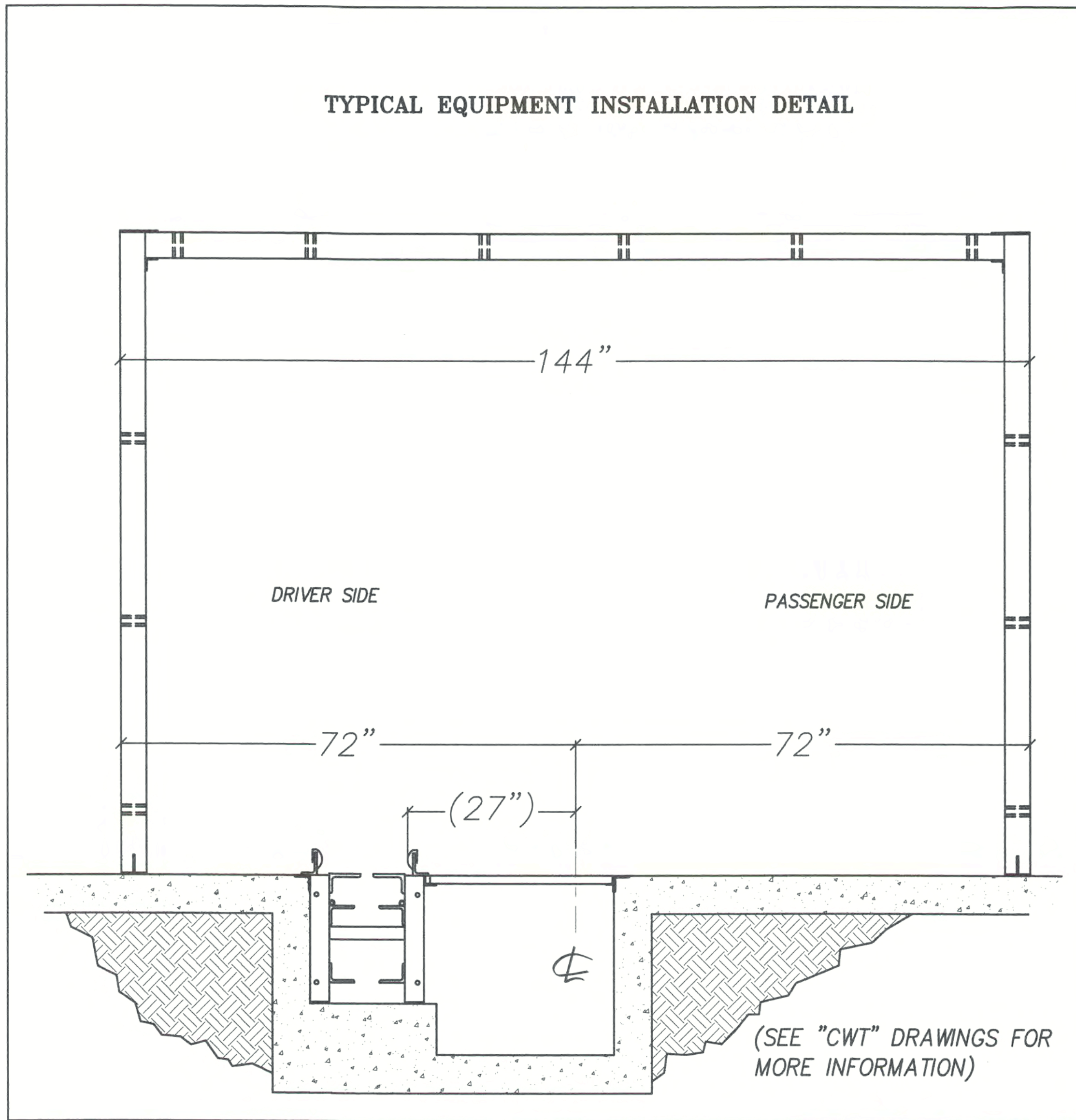
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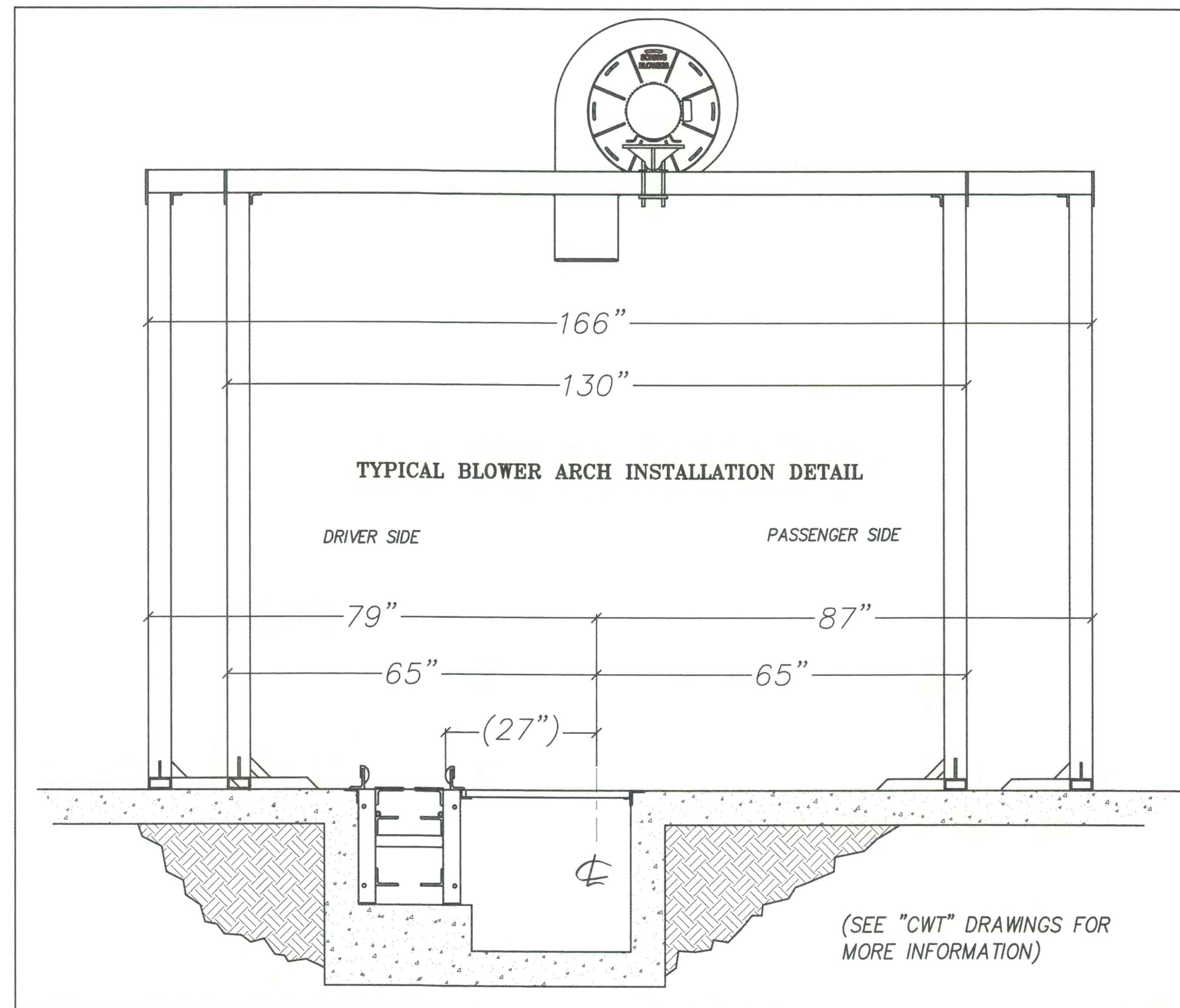
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EQUIPMENT INSTALLATION
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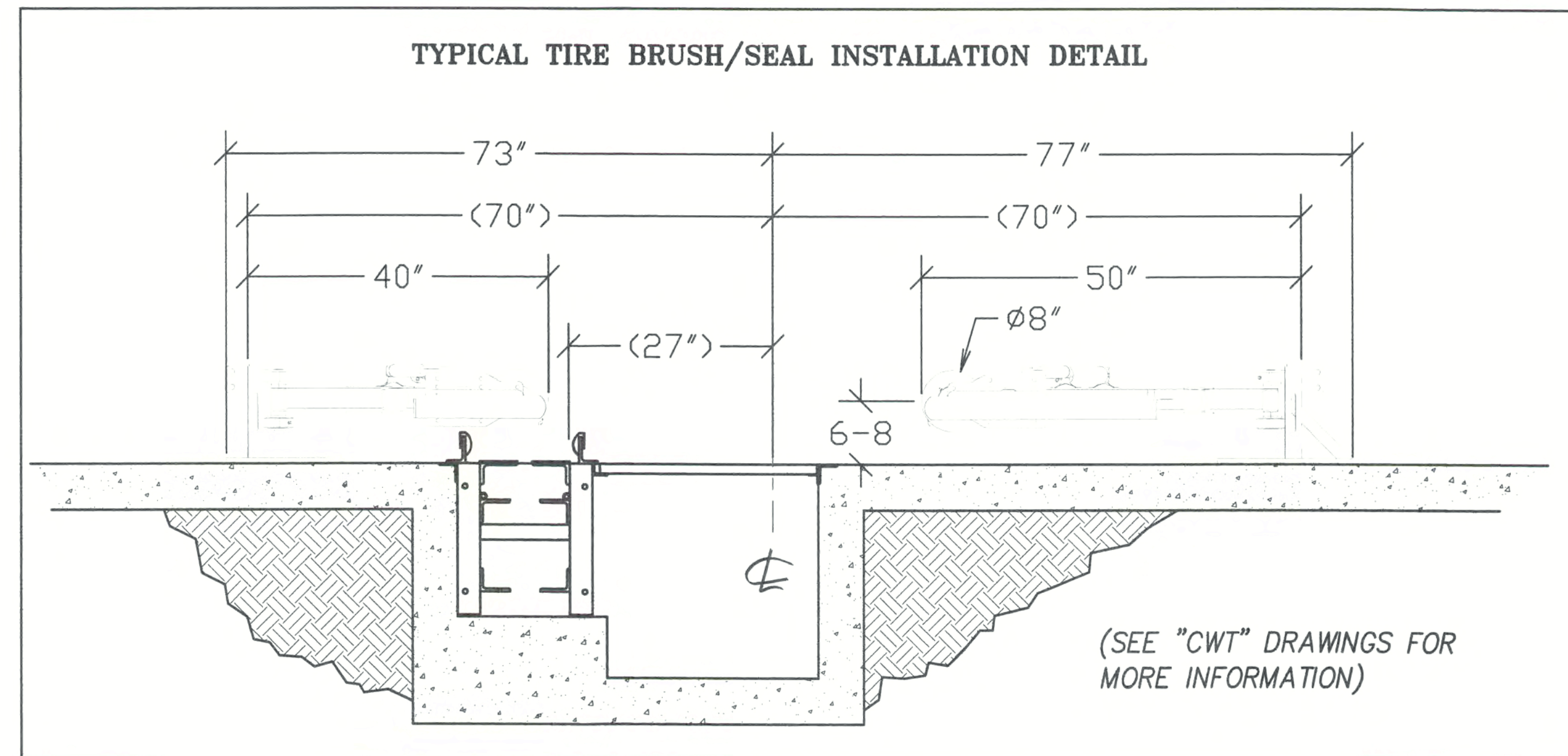
TYPICAL EQUIPMENT INSTALLATION DETAIL



TYPICAL BLOWER ARCH INSTALLATION DETAIL



TYPICAL TIRE BRUSH/SEAL INSTALLATION DETAIL



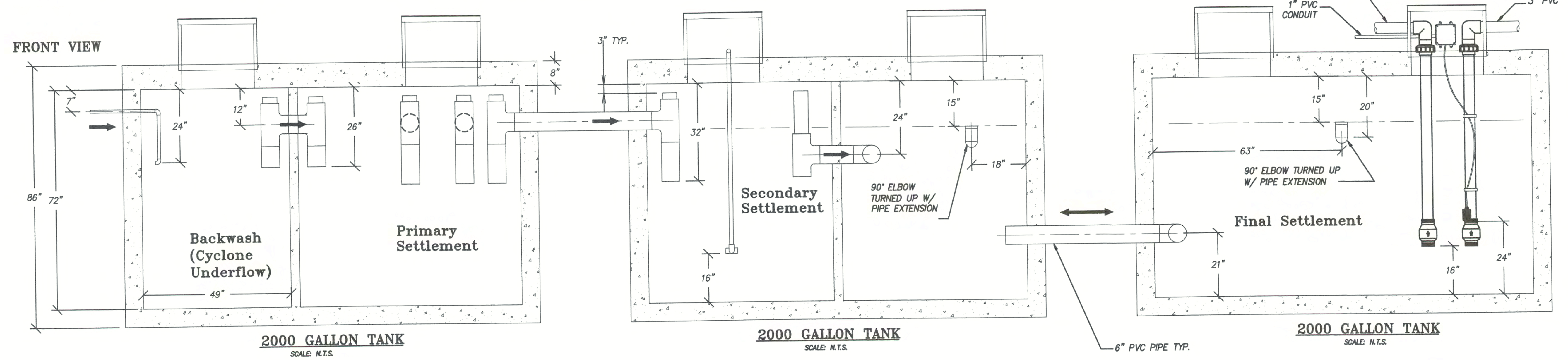
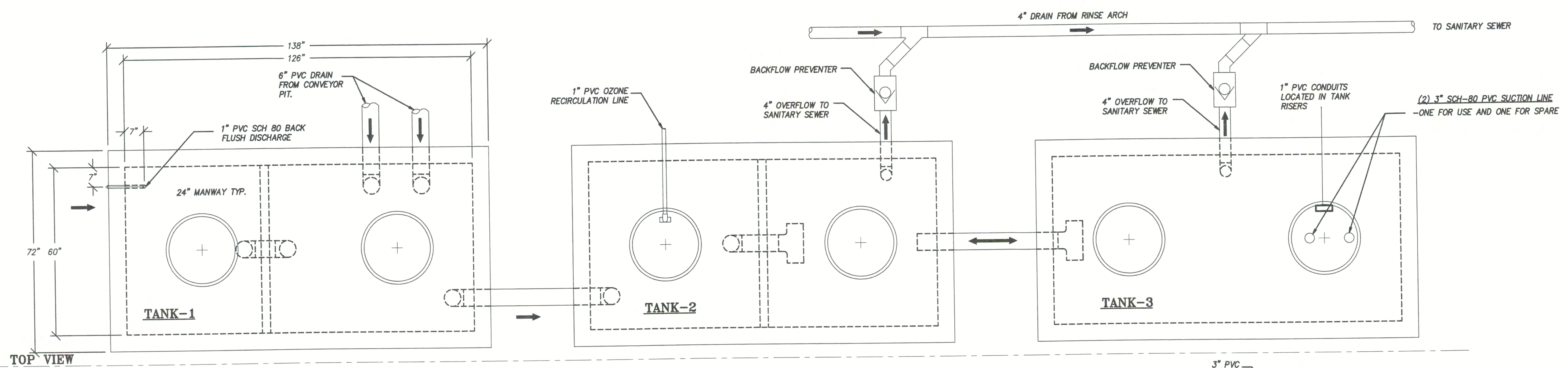
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Sheet	Rev.
L.3	-A-



NOTE:

1. Pressure Test All Suction Lines Upon Installation @ 10-15 PSI max.
2. Keep pressure on Suction Lines for the entire DURATION of construction.
3. Pressure should NOT drop more than 2 PSI.
4. Watch for problems when excavating driveways.
5. Placement of suction lines is critical
6. Do NOT put screens on the suction lines

**CAR WASH RECLAIM SYSTEM
STANDARD RECLAIM 3 TANK**

(SEE RECLAIM SHEET FOR DETAIL)

NOT FOR CONSTRUCTION UNTIL APPROVED BELOW.

APPROVED BY. _____ DATE _____

NOTE:

1. ALL UNDERGROUND TANKS AND PIPING BY CONTRACTOR
2. PIPE LOCATIONS SHOULD BE USED FOR ANY TANK
3. ALL INTERNAL TANK PLUMBING TO BE 6" sch 40 PVC UNLESS NOTED OTHERWISE
4. ALL TANK BOTTOMS SHOULD BE LEVEL
5. THIS DRAWING IS TO SHOW TANK HOLDING CAPACITY AND PIPING RELATIONSHIPS ONLY. THEY ARE NOT INTENDED TO ADDRESS TANK STRUCTURE ENGINEERING.

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RECLAIM TANK SYSTEM; GC

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Sheet	Rev.
P.8	-A-



CITY OF HAYWARD

Hayward City Hall
777 B Street
Hayward, CA 94541
www.Hayward-CA.gov

File #: PH 18-059

DATE: July 12, 2018

TO: Planning Commission

FROM: Planning Manager

SUBJECT

Application to Amend Chapter 10, Article 1(Zoning Ordinance), Sections 10-1.845.j (5) and (6); and 10-2.1045.j(5) and (6) (Minimum Design and Performance Standards) of the Hayward Municipal Code Related to Drive-Through Restaurants and Drive-Through Coffee/Esspresso Shops in the City of Hayward by United Growth Capital Management, LLC. (Applicant), Requiring Approval of a Zoning Text Amendment. Application No. 201802227.

RECOMMENDATION

That the Planning Commission recommend to the City Council approval of the proposed Zoning Text Amendment (Attachment III) to Chapter 10, Article 1, Sections 10-1.845.j (5) and (6); and 10-2.1045.j (5) and (6) (Minimum Design and Performance Standards of the Hayward Municipal Code related to drive-through establishments in the City of Hayward based on the required Findings for Approval (Attachment II).

SUMMARY

United Growth Capital Management, LLC (“United Growth”) is requesting approval of a Zoning Text Amendment (ZTA) application to amend the minimum design and performance standards related to drive-through restaurants and drive-through coffee/espresso shops to allow additional flexibility for the establishment of new drive-through facilities in the City within half-mile of another establishment.

Currently, the minimum design and performance standards for drive-through restaurants and coffee/espresso shops prohibit the establishment of any drive-through restaurant or drive-through coffee/espresso shop within half-mile radius of another establishment as measured from the building walls of existing or proposed buildings. The applicant is requesting to amend the current land use prohibition to allow the establishment of these land uses, if certain additional findings can be made related to location.

ATTACHMENTS

Attachment I	Staff Report
Attachment II	Findings for Approval

File #: PH 18-059

Attachment III	Proposed Text Amendments
Attachment IV	April 2, 2018 CEDC Meeting Minutes
Attachment V	Map of Drive-Through Restaurants
Attachment VI	Maps of Half-Mile Buffer from Freeways



SUBJECT

Application to Amend Chapter 10, Article 1(Zoning Ordinance), Sections 10-1.845.j (5) and (6); and 10-1.1045.j (5) and (6) (Minimum Design and Performance Standards) of the Hayward Municipal Code Related to Drive-Through Restaurants and Drive-Through Coffee/Esspresso Shops in the City of Hayward by United Growth Capital Management, LLC. (Applicant), Requiring Approval of a Zoning Text Amendment. Application No. 201802227.

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SUMMARY

United Growth Capital Management, LLC (“United Growth”) is requesting approval of a Zoning Text Amendment (ZTA) application to amend the minimum design and performance standards related to drive-through restaurants and drive-through coffee/esspresso shops to allow additional flexibility for the establishment of new drive-through facilities in the City within half-mile of another establishment.

Currently, the minimum design and performance standards for drive-through restaurants and coffee/expresso shops prohibit the establishment of any drive-through restaurant or drive-through coffee/esspresso shop within half-mile radius of another establishment as measured from the building walls of existing or proposed buildings. The applicant is requesting to amend the current land use prohibition to allow the establishment of these land uses, if certain additional findings can be made related to location.

BACKGROUND

[United Growth](#) is a development and asset management firm that has specialized in the development and redevelopment of first-class retail centers throughout the United States for over twenty years. The primary objective for United Growth is to attract a productive mix of quality restaurant and retail tenants that serve the community and complement its centers and the surrounding area focusing on trade areas with high traffic, excellent visibility, strong demographics, and the ease of property ingress/egress. United Growth has developed and

managed projects for clients in the past that include, but are not limited to, The Habit Burger Grill, Bank of America, Five Guys, Petco, Starbucks, Panera Bread, and Dick's Sporting Goods.

Regulation History. Research indicates that zoning regulations and ordinances related to drive-in restaurants were originally introduced in the 1980s that restricted the development of these uses. In 1984, an urgency ordinance was adopted prohibiting the development of new drive-in restaurants which might have conflicted with the studies undertaken by the Planning Department to update drive-in restaurant regulations in order to address the problems of traffic circulation, litter, and visual impact created by the proliferation of such uses. Upon completion of the drive-in establishments regulations update in 1986, [Ordinance No. 86-14](#) was introduced and adopted stating that, “[d]rive-in restaurants shall be prohibited within 500-feet of one another when located on and where access thereto is on the same side of a thoroughfare [...]”. Exceptions to this provision existed where drive-in restaurants shared common driveways and there were no other access points from a thoroughfare to the parcel. Since that time, the language was further amended in the 1990s to the City’s current regulations which now specify a half-mile prohibition regardless of which side of the street becoming more restrictive.

Council Economic Development Committee. On April 2, 2018, the applicant presented a preliminary concept review at the [Council Economic Development Committee \(CEDC\) meeting](#), where 4 of the 5 CEDC members were present (Michael Ly absent). The purpose of the review was to introduce the preliminary conceptual project and the related policy issue to the CEDC and obtain high-level feedback related to drive-through establishments, particularly drive-through restaurant uses. During the meeting, each of the CEDC members conveyed their comments, concerns, and questions to United Growth and staff regarding potential model tenants, expectations, operational standards (i.e. debris clean-up), and appropriate locations for additional drive-throughs. The minutes of the CEDC meeting are included as Attachment IV.

Public Outreach. On June 29, 2018, a Notice of this Public Hearing for the Planning Commission meeting was published in The Daily Review. If approved by Council, separate Notices of Application Receipt will be sent for any site-specific location that is proposed to develop a drive-through establishment.

PROJECT DESCRIPTION

Existing Restaurants. Within the City, there are 23 existing drive-through restaurants which include, but are not limited to McDonalds, Burger King, Jack in the Box, Taco Bell and Kentucky Fried Chicken. Attachment V includes a map indicating the drive-through restaurants with their half-mile radii. City staff is currently processing two Conditional Use Permit applications for new Starbucks cafes with one proposed at the new Eden Shores Retail Center and the other at the intersection of Mission Boulevard and Tennyson Road; however, neither application has yet been approved. Based on the map, the majority of the existing drive-through restaurants are focused and concentrated along major arterial streets such as Mission Boulevard, Jackson Street, Hesperian Boulevard, and Tennyson Road. County records indicate that approximately half of the existing drive-through restaurants in the City were developed in the 1980s, before the adoption of the ½ mile prohibition.

As stated previously, the minimum design and performance standards for drive-through restaurants and drive-through coffee/espresso shops are contained within Sections 10-1.845.j(5) and (6) and 10-1.1045.j(5) and (6) of the [Neighborhood Commercial \(CN\)](#) and [General Commercial \(CG\)](#) zoning districts. One of the most significant performance standards for drive-through restaurants is the prohibition of one or more of these establishments (either restaurant or coffee/espresso shop) within a half-mile of each other. As written, the HMC does not provide a mechanism for special findings, circumstances, or deviations from the prohibition such as a Variance. Currently, the Zoning Ordinance only allows for the development of drive-in establishments with the discretionary review and approval of an [Administrative Use Permit \(AUP\)](#) or [Conditional Use Permit \(CUP\)](#). AUPs may be processed administratively with a staff-level decision, whereas CUPs require the review and approval by the Planning Commission – unless otherwise appealed to next decision-making body. Zoning districts that do conditionally permit drive-through restaurants defer to the minimum performance and design standards section of the CN or CG zoning districts for operational and performance criteria; thus, an amendment to these Districts would apply to all applicable districts Citywide that currently allow drive-in and drive-through establishments, as shown in Table 1 below.

Table 1. Hayward Zoning Districts that Conditionally Permit Drive-In Establishments

Zoning District	Entitlement Process
Commercial Neighborhood (CN)	Conditional Use Permit (CUP)
General Commercial (CG)	Conditional Use Permit (CUP)
Central Business (CB)	Conditional Use Permit (CUP)
Commercial Limited (CL)	Conditional Use Permit (CUP)
Central-City Commercial (CC-C)	Conditional Use Permit (CUP)
Industrial District (I)	Administrative Use Permit (AUP)
South Hayward BART and Mission Boulevard Form-Based Codes (Specific Transect Zones)	Conditional Use Permit (CUP)

Proposed Amendment. The proposed amendments are related the minimum design and performance standards for drive-in and drive-through establishments. Specifically, the amendment focuses on the half-mile prohibition to introduce some level of flexibility at select locations in the City, and continue to minimize the placement of new drive-in and drive-through establishments in walkable, pedestrian friendly areas which are not as auto-oriented.

The proposed text amendment (Attachment III) is proposed to maintain the existing half-mile prohibition, but include additional special required findings within the performance standards that will need to be justified in addition to those of the AUP or CUP to allow for an application to be filed with the Planning Division. The required findings to grant flexibility are proposed to include the following:

- i. The drive-in or drive-through restaurants (and coffee/espresso shops) are located within one-half mile from the Interstate 880 (Nimitz Freeway) or State Route 92 (Jackson Freeway) rights-of-way as measured by the existing roadway network;
- ii. The location of the drive-in or drive-through restaurants (and coffee/espresso shops) will not have a substantial adverse effect on vehicular (including bicycle), pedestrian circulation and safety, or transit accessibility;
- iii. The drive-in or drive-through restaurants (and coffee/espresso shops) will not conflict with City adopted goals and policies including, but not limited to, the General Plan and Bicycle Master Plan; and
- iv. The site is suitable and adequate for the proposed use because the drive-in or drive-through restaurant (and coffee/espresso shops) lanes and service windows will be located at least seventy-five (75) feet away from residential uses and residentially zoned properties.

Such flexibility may be exercised if a project site is within a half-mile of the Interstate-880 (Nimitz Freeway) or State Route 92 (Jackson Freeway). It is important to note that the Jackson Freeway commences at the intersection of Santa Clara Street and West Jackson Street going westbound. East of that intersection is not considered the Jackson Freeway, but instead identified as an arterial City street. This finding will ensure that flexibility is granted for locations in proximity of a major freeway that bisect the City, which are consistent with other auto-oriented land uses.

The second and third findings will ensure that any proposed drive-through restaurant will not adversely impact the City's goal for multi-modal transportation options such as driving, biking, and walking. New drive-through restaurants seeking flexibility from the half-mile prohibition shall remain consistent and not conflict with the goals and policies set forth in the City's Hayward 2040 General Plan (Mobility Element), the Bicycle Mater Plan, or the Complete Streets Strategic Initiative. For example, Downtown Hayward is envisioned as a pedestrian-friendly, mixed-use destination where transit accessibility and walkability is prioritized over the automobile. As such, a drive-through restaurant may not be appropriate. Additionally, the proposed findings include a requirement for a 75-foot buffer to minimize impacts of the proposed drive-through activities from adjacent residential properties, including impacts associated with additional light, noise and odors.

In addition to the four new findings being proposed, the project would also need to meet the existing four findings for AUP or CUP issuance:

- a) The proposed use is desirable for the public convenience or welfare;
- b) The proposed use will not impair the character and integrity of the zoning district and surrounding area;
- c) The proposed use will not be detrimental to the public health, safety, or general welfare; and
- d) The proposed use is in harmony with applicable City policies and the intent and purpose of the zoning district involved.

Sustainability Features. The proposed text amendment only includes a modification to the Hayward Municipal Code, but any future drive-in and drive-through establishments would be reviewed to ensure conformance with State and local requirements related to sustainability (i.e. California Building Code). This includes requirements that new development provide a minimal level of energy efficiency, resource conservation, material recycling, air quality, solar readiness, electrical vehicle charging infrastructure, etc.

POLICY CONTEXT AND CODE COMPLIANCE

Hayward 2040 General Plan. The zoning districts that conditionally permit the drive-through restaurants and coffee/espresso shops will not change, and will remain consistent with the accompanying General Plan land use designations that include allow retail, dining, and service uses as either a permitted or conditionally permitted use. Specifically, the City’s General Plan contains policies to ensure that new drive-through establishments do not conflict with, a “variety of topics, including multimodal transportation, regional coordination, complete streets, local circulation, pedestrian facilities, bikeways, public transit, transportation demand management, parking, aviation, goods movement, and transportation funding” (Mobility Element, 2014). If the proposed text amendment is approved, all future development proposals will be evaluated to verify consistency. In addition to the Mobility Element, the proposed text amendment is consistent with the following policies related to Land Use and Community Character as well as the Economic Development Element:

- Land Use Policy LU-5.2 - Flexible Land Use Regulations. To maintain flexible land use regulations that allow the establishment of economically productive uses in regional and community centers.
- Economic Development Policy ED-6.7 - Business Incentives. To provide incentives to attract, expand, and retain businesses that offer high quality jobs, generate local sales tax revenue, and/or provide needed goods or services to residents.

Council Strategic Initiatives. The proposed text amendment will continue to support the goals and objectives of the Complete Streets Strategic Initiative by requiring that all future developments including drive-in and drive-through restaurants or coffee/espresso shops are evaluated to ensure that multi-modal improvements are incorporated into the project, where possible. These multi-modal improvements may include, but may not be limited to the construction, repair, or replacement of sidewalks, bike lanes, crosswalks, curb ramps, and/or streetlights as determined by the nexus of the impact of any development to the public right-of-way. Thus, the project will continue to support the following goal and objective from the Complete Streets Initiative:

Goal 2: Provide complete streets that balance the diverse needs of users of the public right-of-way.

Objective: Increase walking, biking, transit usage, carpooling and other sustainable modes of transportation by designing and retrofitting streets to accommodate all modes.

STAFF ANALYSIS

Staff believes that the Planning Commission can make the required findings (Attachment II) to support the Zoning Text Amendment and recommend approval to the City Council to allow additional flexibility in the location of drive-through restaurants and coffee/espresso shops within the City of Hayward, provided that new establishments conform to the criteria and findings set forth in proposed amendments (Attachment III). These findings will safeguard that new drive-through establishments do not become established in over-concentrated areas of the City or in corridors that do not align with the intent of the zoning district (i.e. Mission Boulevard Corridor Specific Plan Area or Central City Downtown districts). Instead, where appropriate, the drive-through establishments may be allowed to apply close to freeway rights-of-way which already accommodate traffic and trips to and from the freeway. The placement of drive-through restaurants near freeways allows for motorists and patrons to easily access these establishments for the public convenience.

Staff analysis has determined that based on existing zoning districts coupled with the flexibility granted by the proposed text amendment, potential new opportunity sites for drive-in and drive-through restaurants and coffee/espresso shops would be limited to the following areas: Southland Mall area, portions of "A" Street to and from the I-880, West Winton Avenue and Amador Street (adjacent to County buildings), commercial properties adjacent to the SR-92 entrance at West Jackson and Santa Clara Street, and areas of the Industrial zoning district along the SR-92 and I-880. Attachment VI includes a map indicating the half-mile buffer from the Interstate-880 and State-Route 92 freeways with the existing zoning districts that would conditionally permit drive-in establishments.

In addition, through the Use Permit process, City staff would evaluate that all proposed drive-through restaurants be designed to be compatible and sensitive to the adjacent land uses and structures. The CN and CG districts include general location and design criteria standards that exist today which mandate that drive-in uses not be located in close proximity to sensitive receptors such as schools, parks, playgrounds, libraries, churches, and other public or semi-public uses if pedestrian hazards may result.

ENVIRONMENTAL REVIEW

The proposed project is exempt from the California Environmental Quality Act (CEQA) under Section 15061(b)(3), as an activity that is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. The proposed project includes a Zoning Text Amendment to the Hayward Zoning Ordinance that will alleviate prohibitions related to the conditionally permitted locations of drive-through restaurants from each other. Future development projects shall still be required to apply for either an AUP or a CUP and would be evaluated on a case-by-case basis for environmental impacts pursuant to CEQA at that time.

NEXT STEPS

Staff will incorporate input from the Planning Commission and forward the Commission's recommendation to the City Council at a regularly scheduled meeting, tentatively scheduled for July 24, 2018.

Prepared by: Marcus Martinez, Assistant Planner

Recommended by: Jeremy Lochirco, Principal Planner

Approved by:



Sara Buizer, AICP, Planning Manager



Stacey Bristow, Interim Development Services Director

**ZONING TEXT AMENDMENT OF CHAPTER 10, ARTICLE 1(ZONING ORDINANCE), SECTIONS 10-1.845.j (5) and (6); and 10-1.1045.j(5) and (6) (MINIMUM DESIGN AND PERFORMANCE STANDARDS) RELATED TO DRIVE- THROUGH ESTABLISHMENTS
APPLICATION NO. 201802227**

FINDINGS FOR APPROVAL

California Environmental Quality Act

1. The proposed Zoning Text Amendment is exempt from the California Environmental Quality Act (CEQA) under Section 15061(b)(3), as an activity that is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. The proposed Zoning Text Amendment to the Hayward Zoning Ordinance will allow additional flexibility related to the minimum distance requirements of drive-through establishments from one another. Future projects will be subject to additional CEQA review to evaluate any potential environmental impacts associated with that project.

Zoning Text Amendment

Pursuant to Hayward Municipal Code Section 10-1.3425, the Planning Commission may recommend approval of or deny a text amendment to the City Council. Recommendations of approval shall be based upon the following findings:

1. *Substantial proof exists that the proposed change will promote the public health, safety, convenience, and general welfare of the residents of Hayward;*

The proposed Zoning Text Amendment (ZTA) would allow for flexibility, where appropriate, in the locations that currently conditionally permit the development of drive-through restaurants and expresso shops within the City of Hayward. The ZTA would ensure that drive-throughs be located appropriate by including required findings that would be in addition to those of the Administrative Use Permit (AUP) or Conditional Use Permit (CUP). The proposed Text Amendment would provide additional use permit findings that relate to the geographical location of any potential development site for a drive-through restaurant or expresso shop; the proximity of the site to Interstate-880 and State-Route 92; the impacts on private and public circulation; the establishment of adequate buffers from adjacent residential properties; and the consistency with adopted policies related to multi-modal transportation, streets, and mobility. With the inclusion of these findings and additional analysis required to grant greater flexibility, the proposed Amendment will promote public health by limiting the over-concentration of drive-through establishments in residential areas, and mixed-use areas in the City that are focused on improving and expanding bicycle and pedestrian access, such as Mission Boulevard, Foothill Boulevard, and Downtown Hayward.

2. *The proposed change is in conformance with all applicable, officially adopted policies and plans;*

The proposed ZTA would not conflict with the underlying General Plan goals and policies, or the Bicycle Master Plan. The proposed text amendment would support the City's Complete Communities and Complete Streets Strategic Initiative by providing additional flexibility for

establishment of drive-through businesses in the City that would not have a detrimental impact on the multi-modal Complete Streets network. The proposed ZTA includes language and verbiage to ensure and strive for ongoing consistency and compatibility with these documents, stipulating that new drive-through establishments be approved only upon demonstrating that they will not conflict with such long-range goals. Additionally, the proposed ZTA is consistent with the following Land Use and Economic Development General Plan Policies:

- Land Use Policy LU-5.2 - Flexible Land Use Regulations. To maintain flexible land use regulations that allow the establishment of economically productive uses in regional and community centers.
- Economic Development Policy ED-6.7 - Business Incentives. To provide incentives to attract, expand, and retain businesses that offer high quality jobs, generate local sales tax revenue, and/or provide needed goods or services to residents.

3. *Streets and public facilities existing or proposed are adequate to serve all uses permitted when the property is reclassified; and*

No properties are proposed to be reclassified, rezoned, or amended with this application. Rather, the proposed ZTA would introduce flexibility in where drive-through restaurants and expresso shops are conditionally permitted. Such land uses would still be required to obtain the appropriate review and approval of either a AUP or CUP, based on location and zoning district. The determination of whether the streets and public facilities, existing and proposed are adequate to serve the conditionally permitted uses would be reviewed as part of the standard AUP or CUP development review process.

4. *All uses permitted when property is reclassified will be compatible with present and potential future uses, and, further, a beneficial effect will be achieved which is not obtainable under existing regulations.*

No properties are proposed to be reclassified, rezoned, or amended with this application. As stated previously, the proposed ZTA introduces flexibility in where drive-through establishments and expresso shops are conditionally permitted. Currently, the Zoning Ordinance does allow for drive-through establishments as conditional uses. However, the minimum design and performance standards for drive-through restaurants and expresso shops prohibit the establishment of similar facilities within a half-mile from each other – as measured from the exterior walls. The proposed Amendment would maintain the existing distance separation but incorporate a mechanism to allow for additional drive-through restaurants within a half-mile of each other upon meeting the new, required findings aimed to prevent an over-concentration or addition of drive-throughs in areas not deemed appropriate (i.e. Mission Boulevard, Downtown Hayward, Foothill Boulevard).

NEIGHBORHOOD COMMERCIAL (CN) ZONING DISTRICT**SEC. 10-1.845 - MINIMUM DESIGN AND PERFORMANCE STANDARDS.**

The City recognizes that high-quality design of commercial structures can contribute to a positive appearance of commercial districts and neighborhoods and improve the overall character of the community. This Section establishes design and performance standards that shall apply to the construction of residential and commercial buildings and certain commercial uses in the CN District, including but not limited to cultural, educational, religious or recreational facilities. The development of CN-zoned properties in the South of Route 92 planning area is also subject to the provisions of the South of Route 92/Oliver and Weber Properties Specific Plan and the Development Guidelines for the South of Route 92 Oliver/Weber Properties.

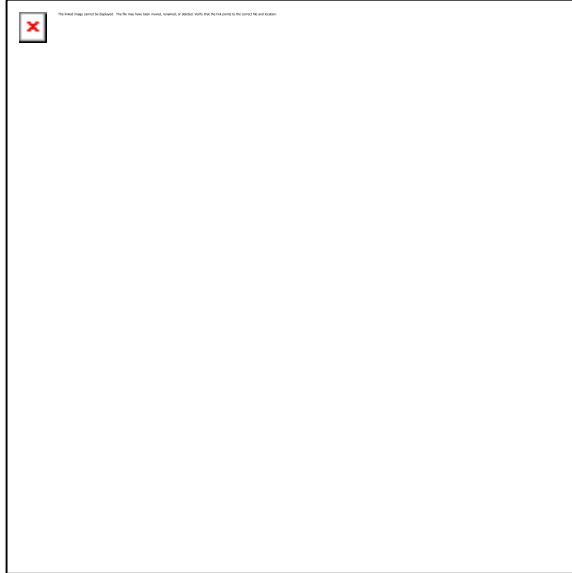
Commercial Buildings and Uses.

For commercial buildings (including second story residential uses) refer to the design criteria contained in the City of Hayward Design Guidelines, the Hillside Design and Urban/Wildland Interface Guidelines and the following specific criteria and standards.

- j. Drive-in Establishments - Special Standards and Conditions.
 - (5) Drive-Through Restaurants.
 - (a) Drive-in or drive-through restaurants shall be prohibited within ½-mile radius of one another as measured from the building walls of existing or proposed buildings, unless all the following required findings are met:
 - i. The drive-in or drive-through restaurants are located within one-half mile from the Interstate 880 (Nimitz Freeway) or State Route 92 (Jackson Freeway) rights-of-way as measured by the existing roadway network;
 - ii. The location of the drive-in or drive-through restaurants will not have a substantial adverse effect on vehicular (including bicycle), pedestrian circulation and safety, or transit accessibility;
 - iii. The drive-in or drive-through restaurants will not conflict with City adopted goals and policies including, but not limited to, the General Plan and Bicycle Master Plan; and
 - iv. The site is suitable and adequate for the proposed use because the drive-in or drive-through restaurant lanes and service windows will be located at least seventy-five (75) feet away from residential uses and residentially zoned properties.
 - (b) For each drive-in restaurant a bicycle rack shall be installed with a capacity for at least five bicycles.
 - (c) Drive-through lanes installed in connection with drive-in restaurants shall have a capacity for at least eight vehicles, at 20 feet per vehicle, unless adequate access and circulation is provided to minimize spillover onto public property.

Attachment III

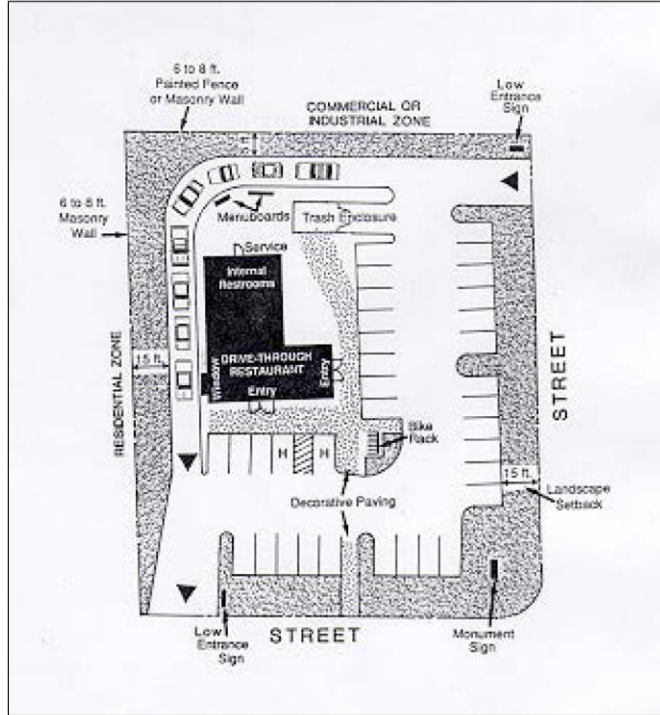
- (d) Pedestrian circulation areas located within drive-in restaurant developments with drive-up windows shall consist of decorative paving such as brick, paving stones, or



Bomanite.

- (e) Access to bathroom facilities located within drive-in restaurant developments shall be from within the structure, with no direct access from the parking area.
- (f) Roof lights, refrigeration units or other extraneous features which are not integral parts of the main structure, inflexible building prototypes which result in an ability to meet setback and compatibility requirements, and unattractive building elevations visible to customers or passersby are prohibited.
- (g) Identification signs for drive-in restaurants (excluding directional signs and the menu board) shall be limited to one monument sign not to exceed 10 feet in height and 36 square feet per face and two wall signs with letters not to exceed 18 inches in height. Total area for wall signs may include logos not to exceed 24 inches in height.
- (h) All required yard areas abutting streets and not used for vehicle maneuvering or parking shall be landscaped. In all zoning districts a landscaped setback at least 15 feet wide shall be installed parallel to the street right(s) of way or precise plan line(s) and on interior property lines where drive-through aisles abut residential zoning districts.
- (i) Minimum building site (lot area or lease area) shall be 25,000 square feet in area, unless adequate access and cross-parking is provided.
- (j) Drive-through aisles shall not be located between the building and the right-of-way and pick-up windows shall not face the right-of-way unless their visibility is minimized through the use of innovative building architecture and mounded or bermed landscaping to minimize their visual impact from the street. Menu board shall be placed so as to not be visible from the street.
- (k) Drive-through restaurants shall have an architectural theme that is unique for a given area as specified by City standards and policies or as determined by the Planning

Director; franchise architecture shall be avoided where possible.



(6) Drive-Through Coffee/Esspresso Shops

(a) Drive-through coffee/esspresso shops shall be prohibited within ½-mile radius of one another as measured from the building walls of existing or proposed buildings, unless all the following required findings are met:

- i. The drive-through coffee/esspresso shops located within one-half mile from the Interstate 880 (Nimitz Freeway) or State Route 92 (Jackson Freeway) rights-of-way as measured by the existing roadway network;
- ii. The location of the drive-through coffee/esspresso shops will not have a substantial adverse effect on vehicular (including bicycle), pedestrian circulation and safety, or transit accessibility;
- iii. The drive-through coffee/esspresso shops will not conflict with City adopted goals and policies including, but not limited to, the General Plan and the Bicycle Master Plan; and
- iv. The site is suitable and adequate for the proposed use because the drive-through coffee/esspresso shop lanes and service windows will be located at least seventy-five (75) feet away from residential uses and residentially zoned properties.

(b) Drive-through coffee/esspresso shop buildings shall not exceed 500 square feet in area.

Attachment III

- (c) Drive-through lanes for drive-up windows shall have a capacity for at least two vehicles, at 20 feet per vehicle, unless adequate access and circulation is provided to minimize spillover onto public property.
- (d) Drive-through lanes shall consist of decorative paving such as brick, paving stones, or Bomanite.
- (e) Public bathroom facilities shall be provided inside the drive-through building, unless public bathroom facilities are located within 200 feet on the same property and are accessible during coffee shop business hours.
- (f) Roof lights, refrigeration units or other extraneous features which are not integral parts of the main structure, inflexible building prototypes which result in an ability to meet setback and compatibility requirements, and unattractive building elevations visible to customers or passersby are prohibited.
- (g) Identification signs for drive-through coffee/espresso shops shall comply with the Hayward Sign Ordinance and shall be limited to one monument sign not to exceed 6 feet in height and two wall signs with letters not to exceed 18 inches in height. Total area for wall signs may include logos not to exceed 24 inches in height.
- (h) All required yard areas abutting streets and not used for vehicle maneuvering or parking shall be landscaped. In all zoning districts a landscaped setback at least 10 feet wide shall be installed parallel to the street right-of-way or precise plan line and on interior property lines where drive-through aisles abut residential zoning districts.
- (i) Minimum building site (lot area or lease area) shall be sufficient to accommodate the building and required circulation, maneuvering and parking.
- (j) Drive-through aisles and pick-up windows may be located between the building and the right-of-way, but their visibility should be minimized through the use of innovative building architecture and mounded or bermed landscaping to minimize their visual impact from the street. Menu board(s) shall be placed so as to not be visible from the street.
- (k) Drive-through coffee/espresso shops shall have an architectural theme that is unique for a given area as specified by City standards and policies or as determined by the Planning Director; franchise architecture shall be avoided where possible.

GENERAL COMMERCIAL (CG) ZONING DISTRICT**SEC. 10-1.1045 - MINIMUM DESIGN AND PERFORMANCE STANDARDS.**

The City recognizes that high-quality design of commercial structures can contribute to a positive appearance of neighborhoods and improve the overall character of the community. This Section establishes design and performance standards that shall apply to residential and commercial development allowed in the CG District, including but not limited to cultural, educational, religious or recreational facilities.

Commercial Buildings and Uses.

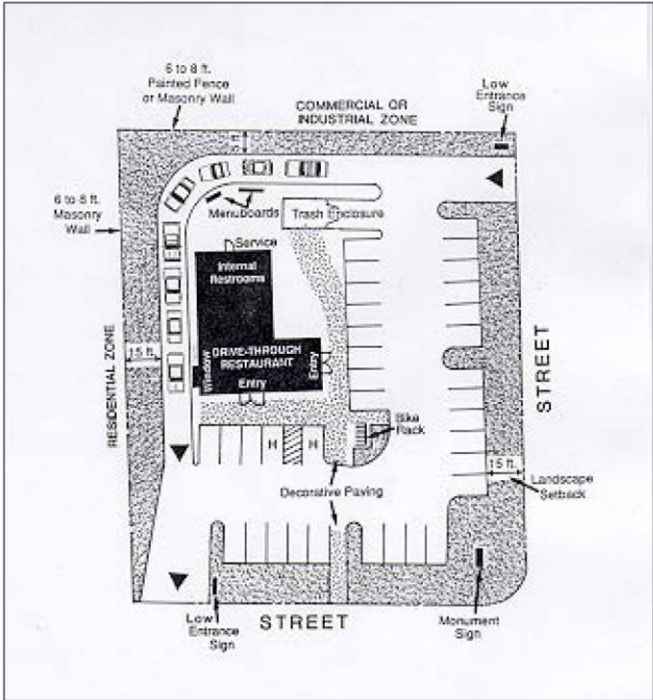
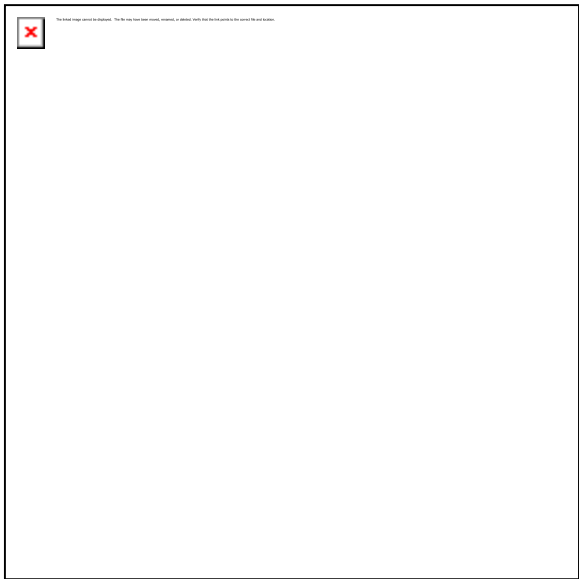
For commercial buildings and use, refer to the design criteria contained in the City of Hayward Design Guidelines, applicable Special Design Districts, the Hillside Design and Urban/Wildland Interface Guidelines and the following specific criteria and standards:

- j. Drive-in Establishments - Special Standards and Conditions.
 - (5) Drive-Through Restaurants.
 - (a) Drive-in or drive-through restaurants shall be prohibited within ½-mile radius of one another as measured from the building walls of existing or proposed buildings, unless all the following required findings are met:
 - i. The drive-in or drive-through restaurants are located within one-half mile from the Interstate 880 (Nimitz Freeway) or State Route 92 (Jackson Freeway) rights-of-way as measured by the existing roadway network;
 - ii. The location of the drive-in or drive-through restaurants will not have a substantial adverse effect on vehicular (including bicycle), pedestrian circulation and safety, or transit accessibility;
 - iii. The drive-through coffee/espresso shops will not conflict with City adopted goals and policies including, but not limited to, the General Plan and the Bicycle Master Plan; and
 - iv. The site is suitable and adequate for the proposed use because the drive-in or drive-through restaurant lanes and service windows will be located at least seventy-five (75) feet away from residential uses and residentially zoned properties.
 - (b) For each drive-in restaurant a bicycle rack shall be installed with a capacity for at least five bicycles.
 - (c) Drive-through lanes installed in connection with drive-in restaurants shall have a capacity for at least eight vehicles, at 20 feet per vehicle, unless adequate access and circulation is provided to minimize spillover onto public property.
 - (d) Pedestrian circulation areas located within drive-in restaurant developments with drive-up windows shall consist of decorative paving such as brick, paving stones, or Bomanite.
 - (e) Access to bathroom facilities located within drive-in restaurant developments shall be from within the structure, with no direct access from the parking area.
 - (f) Roof lights, refrigeration units or other extraneous features which are not integral parts of the main structure, inflexible building prototypes which result in an ability to meet

Attachment III

setback and compatibility requirements, and unattractive building elevations visible to customers or passersby are prohibited.

- (g) Identification signs for drive-in restaurants (excluding directional signs and the menu board) shall be limited to one monument sign not to exceed 10 feet in height and 36 square feet per face and two wall signs with letters not to exceed 18 inches in height. Total area for wall signs may include logos not to exceed 24 inches in height.
- (h) All required yard areas abutting streets and not used for vehicle maneuvering or parking shall be landscaped. In all zoning districts a landscaped setback at least 15 feet wide shall be installed parallel to the street right(s) of way or precise plan line(s) and on interior property lines where drive-through aisles abut residential zoning districts.
- (i) Minimum building site (lot area or lease area) shall be 25,000 square feet in area, unless adequate access and cross-parking is provided.
- (j) Drive-through aisles shall not be located between the building and the right-of-way and pick-up windows shall not face the right-of-way unless their visibility is minimized through the use of innovative building architecture and mounded or bermed landscaping to minimize their visual impact from the street. Menu board shall be placed so as to not be visible from the street.
- (k) Drive-through restaurants shall have an architectural theme that is unique for a given area as specified by City standards and policies or as determined by the Planning Director; franchise architecture shall be avoided where possible.



- (6) Drive-Through Coffee/Espresso Shops
 - (a) Drive-through coffee/espresso shops shall be prohibited within 1/2-mile radius of one another as measured from the building walls of existing or proposed buildings, unless all the following required findings are met:
 - i. The drive-through coffee/espresso shops located within one-half mile from the Interstate 880 (Nimitz Freeway) or State Route 92 (Jackson Freeway) rights-of-way as measured by the existing roadway network;
 - ii. The location of the drive-through coffee/espresso shops will not have a substantial adverse effect on vehicular (including bicycle), pedestrian circulation and safety, or transit accessibility;

Attachment III

- iii. The drive-through coffee/espresso shops will not conflict with City adopted goals and policies including, but not limited to, the General Plan and the Bicycle Master Plan; and
 - iv. The site is suitable and adequate for the proposed use because the drive-through coffee/espresso shop lanes and service windows will be located at least seventy-five (75) feet away from residential uses and residentially zoned properties.
- (b) Drive-through coffee/espresso shop buildings shall not exceed 500 square feet in area.
 - (c) Drive-through lanes for drive-up windows shall have a capacity for at least two vehicles, at 20 feet per vehicle, unless adequate access and circulation is provided to minimize spillover onto public property.
 - (d) Drive-through lanes shall consist of decorative paving such as brick, paving stones, or Bomanite.
 - (e) Public bathroom facilities shall be provided inside the drive-through building, unless public bathroom facilities are located within 200 feet on the same property and are accessible during coffee shop business hours.
 - (f) Roof lights, refrigeration units or other extraneous features which are not integral parts of the main structure, inflexible building prototypes which result in an ability to meet setback and compatibility requirements, and unattractive building elevations visible to customers or passersby are prohibited.
 - (g) Identification signs for drive-through coffee/espresso shops shall comply with the Hayward Sign Ordinance and shall be limited to one monument sign not to exceed 6 feet in height and two wall signs with letters not to exceed 18 inches in height. Total area for wall signs may include logos not to exceed 24 inches in height.
 - (h) All required yard areas abutting streets and not used for vehicle maneuvering or parking shall be landscaped. In all zoning districts a landscaped setback at least 10 feet wide shall be installed parallel to the street right-of-way or precise plan line and on interior property lines where drive-through aisles abut residential zoning districts.
 - (i) Minimum building site (lot area or lease area) shall be sufficient to accommodate the building and required circulation, maneuvering and parking.
 - (j) Drive-through aisles and pick-up windows may be located between the building and the right-of-way, but their visibility should be minimized through the use of innovative building architecture and mounded or bermed landscaping to minimize their visual impact from the street. Menu board(s) shall be placed so as to not be visible from the street.
 - (k) Drive-through coffee/espresso shops shall have an architectural theme that is unique for a given area as specified by City standards and policies or as determined by the Planning Director; franchise architecture shall be avoided where possible.



COUNCIL ECONOMIC DEVELOPMENT COMMITTEE

MEETING MINUTES – April 2, 2018

CALL TO ORDER: Mayor Halliday called the meeting to order at 4:00 p.m.

ATTENDANCE:

Committee Member	Present 4/2/18	All Meetings Year to Date		Meetings Mandated By Resolution	
		Present	Absent	Present	Absent
Michael Ly		3	2	3	2
Didacus-Jeff Joseph Ramos	✓	4	1	4	1
Mayor Halliday (Chair)	✓	5	0	5	0
Council Member Márquez	✓	5	0	5	0
Council Member Mendall	✓	5	0	5	0

OTHERS IN ATTENDANCE:

Maria Hurtado, Assistant City Manager; Stacey Bristow, Interim Director of Development Services; Micah Hinkle, Economic Development Manager; Paul Nguyen, Economic Development Specialist; Ramona Thomas, Economic Development Specialist; Marcus Martinez, Assistant Planner; Suzanne Philis, Senior Secretary; from United Growth: CEO Brad LaRue, Vice President of Capital Management Carmelita Botelho, and Development Manager Futaba Alizoti

PUBLIC COMMENTS

Hayward Chamber of Commerce President and CEO Kim Huggett distributed a By the Numbers flyer that highlighted Chamber accomplishments. Mr. Huggett noted 40 free small business workshops had been held in the last 40 months (many in cooperation with the Alameda County Small Business Development Center and City of Hayward), and that export certificates had been sent to 50 countries.

Mayor Halliday announced that Hayward won two out of eight categories at the East Bay Economic Development Alliance Innovation Awards on Thursday, March 29th at the Fox Theater in Oakland. She said Therm-x (maker of custom components for the semi-conductor industry) won the Advanced Manufacturing category, and Reflexion (developing a revolutionary device for the detection and prevention of cancer) won the Life Sciences category. She commented that Hayward has had finalists for the last three years. Council Member Mendall pointed out that Hayward was the only Bay area city to have two winners.

Hayward Council Economic Development Committee
Regular Meeting Minutes
April 2, 2018
Page 2 of 5

1. APPROVAL OF MINUTES OF SPECIAL MEETING MARCH 5, 2018

A motion to approve minutes with one minor change was made by Council Member Márquez with a second by Council Member Mendall. Minutes from the March 5, 2018 Regular Meeting were approved with Member Ly absent.

Staff distributed comments from Member Ramos on the Regional Minimum Wage Inventory which was presented and discussed at the March 5th meeting. Member Ramos was unable to attend the meeting.

2. PRELIMINARY CONCEPT REVIEW – UNITED GROWTH DRIVE THROUGH – 2429 WHIPPLE ROAD

Economic Development Manager Hinkle introduced the project noting the feedback desired was also a review of the City's drive-thru policy.

Economic Development Specialist Thomas said United Growth Capital Management specialized in the development and redevelopment of first-class retail centers and noted they were interested in building a drive-thru at Wiegman and Whipple Roads in the Industrial area, but current regulations prohibited two drive-thru restaurants being located within a half-mile of one another. She said the proposed location was within the 880 Retail Area and provided a map displaying existing drive-thru restaurants and their half-mile radii.

Assistant Planner Martinez explained that under the General Commercial zoning district an applicant was prohibited from applying for an Administrative Use permit to build a drive-thru restaurant because two drive-thru restaurants could not be located within a half-mile of one another. He said staff was seeking high-level feedback related to approval of a Zoning Text Amendment that would allow a "special required finding" for this and future drive-thru restaurants to locate within that half-mile radius.

United Growth Vice President of Capital Management Carmelita Botelho explained that because the cost of doing business was so expensive in California, they were having trouble finding a non-drive-thru tenant for the site. She said United Growth was seeking feedback to determine if the site was worth pursuing.

United Growth CEO Brad LaRue said they had been working hard to find a tenant at this site for the last four years. He said that Panera Bread, Noodles & Co., and Habit Burger had all opted out when a drive-thru was not an option.

Council Member Mendall commented that the provided leakage data (type of retail businesses that weren't already present in the area) didn't match past reports. Manager Hinkle explained that the data provided by United Growth was for a 5-mile radius from the site, not city-wide.

Council Member Mendall said that, in general, he was pleased that Hayward made it difficult for drive-thru restaurants because most offered unhealthy food, paid low wages, and generated tons of litter. He said he would relax the City's hard no and consider a variance, but acknowledged that that would generate many applications so he wanted to make his expectations clear.

Hayward Council Economic Development Committee
Regular Meeting Minutes
April 2, 2018
Page 3 of 5

Council Member Mendall listed the following expectations: 1) The incoming drive-thru restaurant must be something new; either the first of its kind in Hayward, or maybe the second; 2) The litter component must be resolved so the City alone wasn't responsible for the cost and process of clean-up; and 3) The restaurant offered higher base wages, for example \$15 regardless of the City rate. If all three expectations were met, Council Member Mendall said he would enthusiastically vote to approve the business.

Council Member Márquez asked if a Text Amendment would change the City's zoning code. Assistant Planner Martinez said yes, the Amendment would have City-wide implications, but for this request, site specific approval. Economic Development Manager Hinkle noted the Text Amendment could be a driver for auto-oriented locations not near neighborhoods but in existing high traffic areas.

Assistant Planner Martinez explained that if the Text Amendment was adopted, drive-thru restaurants would still not be permitted by-right, but could seek approval.

Council Member Márquez asked about the timeline for this project and Assistant Planner Martinez said the Text Amendment would have to be approved first. Interim Director of Development Services Bristow said that would take approximately three to four months. Council Member Márquez asked if the Amendment would go straight to Council and she was told the Planning Commission would review first.

Economic Development Manager Hinkle noted there were only two locations still available in the City that allowed a drive-thru restaurant by-right and Starbucks had submitted applications for both locations.

Council Member Márquez commented that the site on Wiegman was not visible from I-880 and she asked about signage. Interim Director of Development Services Bristow said standard sign regulations would apply to the site or United Growth could ask for a variance. Assistant Planner Martinez said the sign could be bigger because of the location. CEO LaRue said a monument sign had been requested on an earlier proposal.

Council Member Márquez said for her approval the restaurant would have to offer healthy choices. She asked where the nearest drive-thru Panera Bread was located and was told Concord. She said Panera would be the type of restaurant she would approve.

Member Ramos asked why a half-mile radius was used instead of a quarter-mile. Assistant Planner Martinez said the regulations were carried forward from the 80s or 90s and should perhaps be reviewed. Interim Director of Development Services Bristow noted the Industrial regulations were currently being updated.

Member Ramos asked if the regulations needed to be uniform and Interim Director of Development Services Bristow said no.

Member Ramos said that he liked that the proposed drive-thru wouldn't impact traffic on Whipple Road. He said he would support a Panera at the site and then commented that communities don't usually name the tenant. He asked if anything prohibited the City from naming preferred tenants. Council Member Mendall said that was why he was focusing on preferred criteria.

Hayward Council Economic Development Committee
Regular Meeting Minutes
April 2, 2018
Page 4 of 5

Member Ramos said the half-mile radius could be adjusted, but the staff report's other proposed restrictions were valid. He said he would prefer a mom n' pop business or a franchise not already in California; he wanted something unique for Hayward. He said the design of the proposal was fine. Member Ramos said some flexibility at this site might benefit the City.

Mayor Halliday commented that although the City was working regionally to be more walkable she acknowledged a drive-thru was appropriate for the already high-traffic area and that United Growth had been unsuccessful in finding a non-drive-thru tenant.

Mayor Halliday noted that Hayward already had a Panera, although it wasn't a drive-thru, and said she also preferred a restaurant with healthy choices. She asked about vegan restaurants like Amy's Kitchen and noted the closest one was in Corte Madera.

Mayor Halliday said staff should work to craft regulations that didn't open the door too wide for drive-thru restaurants and agreed with Council Member Mendall that litter was a problem. She said the City knew about litter and frequently organized clean ups. Member Ramos commented that trash from fast food restaurants was not anonymous.

Mr. LaRue said they could place more garbage cans on the site to make it more convenient for patrons to throw away their trash. Members said that may not be enough and discussed the problem of litter generated by drive-thru restaurants. Member Ramos noted the McDonalds downtown had employees pick up trash in the area around the restaurant.

Council Member Márquez mentioned another restaurant she would consider for the site. Ms. Botelho commented that most mom n' pop establishments couldn't afford the rents associated with a new development. She said it might be affordable if the project entailed the rehab of an existing building.

Mr. LaRue asked for confirmation that there was enough support for a drive-thru and said the City could craft the language that created the subjectivity for approvals.

Mayor Halliday confirmed the restaurant would also have the option to sit down to eat rather than drive-thru. Mr. LaRue said a drive-thru would allow the tenant to supplement revenues and afford the market rent. Ms. Botelho noted workers in the area might appreciate having the choice of driving thru when they are in a hurry, sitting when they have more time, and enjoying patio seating on a nice day.

Mr. LaRue noted because the site was small there were limited options and asked the Committee for leniency noting United Growth had already spent a lot of money on the site.

Council Member Mendall noted the Jack in the Box at Fairway Plaza, in trade for Council's approval to stay open 24 hours, had an agreement in place that required them to pick up litter for a quarter of a mile along the par course across Mission Boulevard from the restaurant. He suggested that arrangement be used as a model.

Mayor Halliday said the City appreciated the efforts of United Growth and acknowledged the site was different because of the auto-intensity of the area. She said the City wanted to work with United Growth to find a tenant, noted only three Council Members were present, but thought other Council Members would have similar concerns and comments. Council Member Mendall agreed.

Hayward Council Economic Development Committee
Regular Meeting Minutes
April 2, 2018
Page 5 of 5

3. FUTURE MEETING TOPICS AS OF APRIL 2, 2018

In response to a request made by Member Ramos, Economic Development Manager Hinkle presented a table prepared by Senior Secretary Philis that described three different types of Bucks Programs; incentive programs that encouraged local spending.

Member Ramos said the table was exactly what he was looking for and commented that community-run programs like the Downtown Hayward Passport Program, which the City started but handed off to local merchants, had been very successful. He said the examples provided were more money-oriented and suggested neighborhood-driven programs. Member Ramos said some areas don't feel like part of Hayward and Bucks programs could help unify the City.

Council Member Mendall said he still wanted to see a report on the changing demand of retail tied into an analysis of sales tax revenues generated by housing versus retail uses. Manager Hinkle said he was trying to produce those reports.

Mayor Halliday said she liked the idea of improved marketing of local events (#2 on the list) because it would tie in with the City's commitment to recognize different cultures.

Council Member Márquez asked what was on the schedule for May. Manager Hinkle said the agenda was still being developed, but he hoped to present a market trend analysis of retail and continue down the list of Meeting Topics.

Member Ramos acknowledged the Economic Development Division was down staff, but he suggested a catalog or pocket guide of local retail to help introduce local businesses to new people.

Member Ramos also commented that although the Makerspace was a good place to explore a new business, and the workshops provided by the Chamber were helpful to small business, people needed a place where they can learn how to start a new business and then start it.

COMMITTEE MEMBER ANNOUNCEMENTS AND REFERRALS

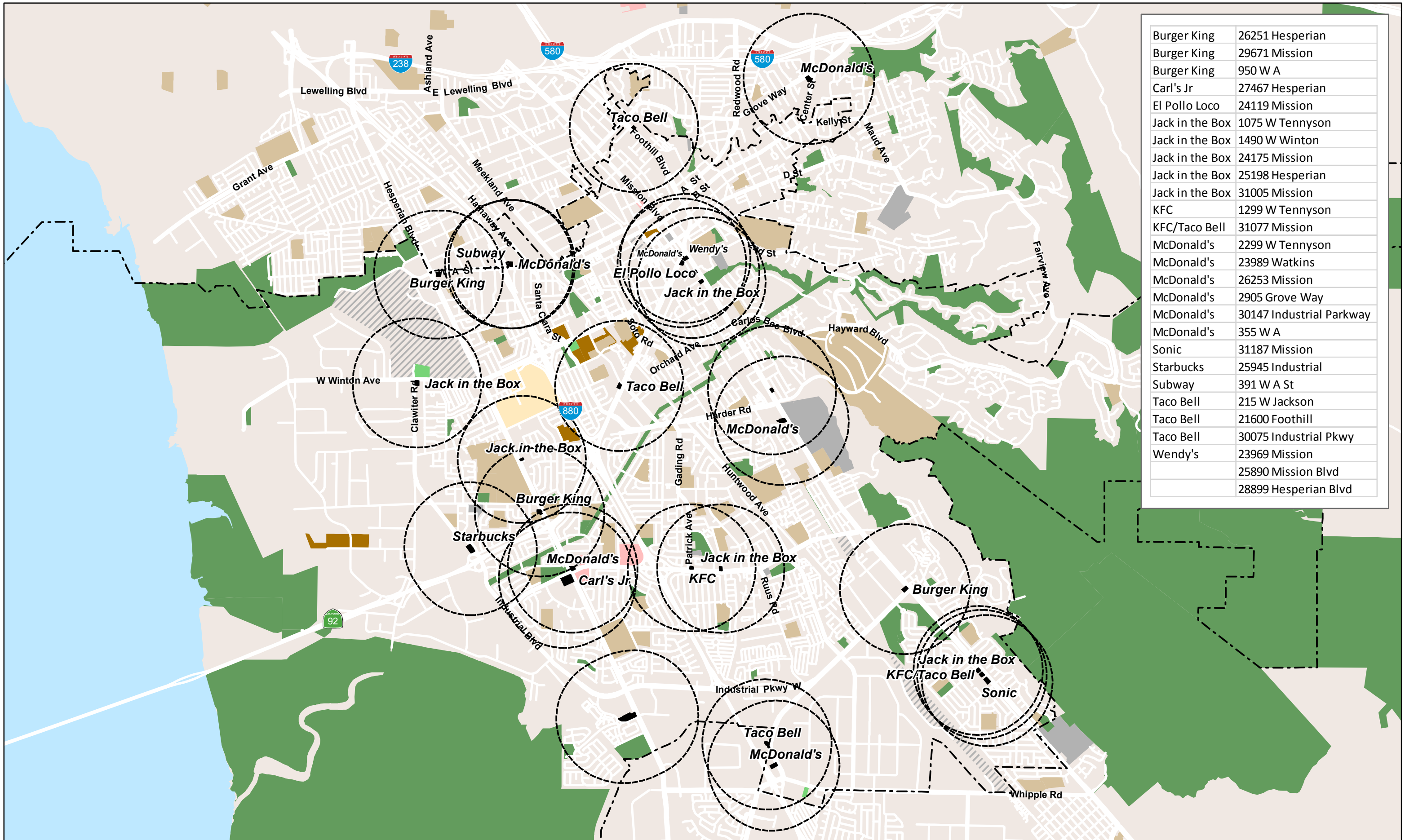
Economic Development Manager Hinkle announced that today's meeting was the last for Economic Development Specialist Nguyen who had accepted the Economic Development Manager position with the City of Fairfield. He said the City was sorry to see Specialist Nguyen leave, but understood the desire to advance.

Specialist Nguyen thanked Committee members, and the City, for the opportunity to serve as Industrial Specialist. He noted in the three years and seven months since he'd joined the team he had assisted 114 businesses, attracted 40 to Hayward, supported the addition of 819 jobs and facilitated real estate investments totaling \$136,017,388.

Council Member Mendall and Mayor Halliday said they were very sorry to see him go.

Chamber President Huggett said the City must create an equal and higher value replacement.

ADJOURNMENT: The meeting was adjourned at 5:19 p.m.



Burger King	26251 Hesperian
Burger King	29671 Mission
Burger King	950 W A
Carl's Jr	27467 Hesperian
El Pollo Loco	24119 Mission
Jack in the Box	1075 W Tennyson
Jack in the Box	1490 W Winton
Jack in the Box	24175 Mission
Jack in the Box	25198 Hesperian
Jack in the Box	31005 Mission
KFC	1299 W Tennyson
KFC/Taco Bell	31077 Mission
McDonald's	2299 W Tennyson
McDonald's	23989 Watkins
McDonald's	26253 Mission
McDonald's	2905 Grove Way
McDonald's	30147 Industrial Parkway
McDonald's	355 W A
Sonic	31187 Mission
Starbucks	25945 Industrial
Subway	391 W A St
Taco Bell	215 W Jackson
Taco Bell	21600 Foothill
Taco Bell	30075 Industrial Pkwy
Wendy's	23969 Mission
	25890 Mission Blvd
	28899 Hesperian Blvd



880 & 92 - Half Mile Buffer with Zoning

